

**Town of Kittery  
 Planning Board Meeting  
 April 11, 2019**

**ITEM 2 - Huntington Run Cluster Subdivision – Major Modification to an Approved Subdivision Plan**

Action: Approve or deny major modification. Owner / applicant, Chinburg Development, LLC requests consideration of a major modification to an approved plan to amend the roadway design for a 20-lot cluster subdivision on 86.6 +/- acres located on Betty Welch Road (Tax Map 66 Lots 2A, 8 & 8A) in the Residential Rural Zone (R-RL) and a portion located in the Shoreland Overlay (SH-250’-OZ) Zone. Agent is Jeff Clifford, P.E., Altus Engineering.

**PROJECT TRACKING**

REQ'D	ACTION	COMMENTS	STATUS
Yes	Sketch Plan Review / Concept Approval	4/13/2017	APPROVED
No	Site Visit	8/15/2017	HELD
Yes	Preliminary Plan Review Completeness/Acceptance	7/13/2017	ACCEPTED
Yes	Public Hearing	8/24/2017.	HELD
Yes	Preliminary Plan Approval	8/24/2017	APPROVED
Yes	Final Plan Review	2/14/2019; 3/14/2019	APPROVED
<p><b>Prior to the signing of the approved Plan any Conditions of Approval related to the Findings of Fact along with waivers and variances (by the BOA) must be placed on the Final Plan and, when applicable, recorded at the York County Registry of Deeds. PLACE THE MAP AND LOT NUMBER IN 1/4" HIGH LETTERS AT LOWER RIGHT BORDER OF ALL PLAN SHEETS. Per Section 16.4.4.L - Grading/Construction Final Plan Required. - Grading or construction of roads, grading of land or lots, or construction of buildings is prohibited until the original copy of the approved final plan has been duly recorded in the York County registry of deeds, when applicable.</b></p>			

**Background**

The Board approved the final plan(s) for this proposed subdivision at the March 14, 2019 meeting. At that meeting, the applicant presented a waiver request from the Design and Construction Standards for Streets and Pedestrian Ways (Table 1) to provide 1-foot wide paved shoulders in lieu of the 2-foot and 8-foot wide paved shoulder standard listed in Table 1. The Board did not approve the waiver request as presented. It cited as a concern the safety of children traveling along Huntington Way to the school bus stop at Betty Welch Road.

To address that concern, the applicant has submitted a request to amend the approved plan by seeking a waiver from the paved shoulders standard to provide a 4 foot wide paved shoulder on the south side of Huntington Way from its intersection with Betty Welch Road to the beginning of the cul-de-sac loop (adjacent to lot 19), a distance of 935 feet. Striping will delineate the paved shoulder from the paved travel way. All other roadway shoulders are proposed to be constructed with 1-foot of pavement and gravel shoulders of varying width as depicted on the plan.

**Staff Review**

The applicant is requesting a major modification to an approved plan as allowed under Section 16.10.9.3. Modifications to approved plan for the proposed roadway design. The waiver request is from 16.8.4.4 and Table 1 Class III Private Streets Standard – Street Width Design: d. Paved Shoulder where the requirement is 2/8 feet, walk side/opp. side. No other waivers are necessary.

Staff has reviewed the proposed modification with the appropriate department heads – DPW, Police and Fire as well as the Sewer Superintendent at a Technical Review Committee meeting on March 26<sup>th</sup>. In addition, staff has discussed the roadway modification with CMA Engineers. The consensus is that the proposed roadway design is appropriate for the level of traffic and the safety of the users of the road.

**Recommendation / Action**

*Approve the following waiver from Title 16 and major modification to the approved plan:*

- 1. Section 16.8.4.4 and Table 1 Class III Private Streets Standard. Street Width Design: d. Paved Shoulder. A 4 foot wide paved shoulder on the south side of Huntington Way from its intersection with Betty Welch Road to the beginning of the cul-de-sac loop (adjacent to Lot 19). All other roadway shoulders are proposed as 1 foot of pavement with gravel shoulders of varying width as depicted on the plan.*



***Town of Kittery, Maine***  
***Department of Public Works***  
***200 Rogers Road, Kittery, ME 03904***  
***Telephone: (207) 439-0333 Fax: (207) 439-6118***

***MEMO***

**To:** Adam Causey, Planning/ Development Director  
**From:** David Rich, Commissioner of Public Works  
**Subject:** Huntington Run Site Plan  
**Date:** April 3, 2019

Adam:

Per Title 16.8 Table 1, this development falls under Minor Street per the ADT count. The developer has requested several major waivers for the project. The current waivers require the street to remain private. I see no issue with the current paved shoulder request.

**From:** [David O'Brien](#)  
**To:** [Jamie Steffen](#)  
**Subject:** Re: TRC Comment on Huntington Run Roadway Design  
**Date:** Wednesday, April 03, 2019 5:35:40 PM

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Thank you. FD is okay with the changes

Sent from my iPhone

On Apr 3, 2019, at 5:25 PM, Jamie Steffen <[JSteffen@kitteryme.org](mailto:JSteffen@kitteryme.org)> wrote:

Dave,

Here are the roadway plans that have the hydrant locations. There is the existing one on Betty Welch Road at the entrance and the new one in the cul-de-sac loop. They are not changing with this modification.

Thanks,

Jamie

Jamie Steffen  
Town Planner  
Town of Kittery  
200 Rogers Road  
Kittery, ME 03904  
1-207-475-1323  
[jsteffen@kitteryme.org](mailto:jsteffen@kitteryme.org)

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**From:** David O'Brien  
**Sent:** Wednesday, April 03, 2019 4:40 PM  
**To:** Jamie Steffen <[JSteffen@kitteryme.org](mailto:JSteffen@kitteryme.org)>  
**Subject:** RE: TRC Comment on Huntington Run Roadway Design

I need to see the drawings showing that hydrant locations have not changed

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**From:** Jamie Steffen  
**Sent:** Wednesday, April 03, 2019 10:28 AM  
**To:** Adam Causey <[ACausey@kitteryme.org](mailto:ACausey@kitteryme.org)>; David O'Brien <[DO'Brien@kitteryme.org](mailto:DO'Brien@kitteryme.org)>; David Rich <[drich@kitteryme.org](mailto:drich@kitteryme.org)>; Robert Richter <[rrichter@kitterypolice.com](mailto:rrichter@kitterypolice.com)>; Timothy Babkirk <[TBabkirk@kitteryme.org](mailto:TBabkirk@kitteryme.org)>; William Straub <[wstraub@cmaengineers.com](mailto:wstraub@cmaengineers.com)>  
**Cc:** Jessa Kellogg <[JKellogg@kitteryme.org](mailto:JKellogg@kitteryme.org)>  
**Subject:** TRC Comment on Huntington Run Roadway Design

Hi all,

As I explained at our TRC meeting, this is going back before the Planning Board at their April 11<sup>th</sup> meeting for a major modification to an approved plan. I would like to have your comments in writing on this latest proposal for the Planning Board's consideration. I'm preparing their packets tomorrow afternoon so if you can get me those by then I would appreciate it.

Tim, I don't need anything from you on this even though you were at the meeting.

Thank you,

Jamie

Jamie Steffen

Town Planner

Town of Kittery

200 Rogers Road

Kittery, ME 03904

1-207-475-1323

[jsteffen@kitteryme.org](mailto:jsteffen@kitteryme.org)

<Roadway Plan and Profile - A.pdf>

<Roadway Plan and Profile - B.pdf>



April 3, 2019

Jamie Steffen, Town Planner  
Town of Kittery  
P.O. Box 808  
Kittery, Maine 03904

**RE: Town of Kittery, Planning Board Services  
Huntington Run Cluster Subdivision; 40 Betty Welch Road, Tax Map 66, Lots 2A, 8, & 8A  
Review #3. CMA #591.108**

Dear Jamie:

We have reviewed the application for major modification to an approved plan prepared for the Huntington Run Cluster Subdivision by Altus Engineering. The subdivision was approved at the March 14, 2019 planning board meeting, with the Town's standard roadway cross section included in the design. Going into that meeting, Altus had proposed a waiver of the road standards that was somewhat similar to what is included in the current application for major modification.

The current application includes paved shoulders on both sides of the road, in addition to the paved travel way (10' lanes). We concur with that design as it is consistent with the recommendation we made in our previous reviews of the project regarding paved shoulders. On one side (north side), the paved shoulder is 1 -ft. wide, with an additional 2-feet of gravel shoulder. On the other side (south side) a 4-foot paved shoulder is proposed as a separate pedestrian way, with an additional 1-foot gravel shoulder. The total roadway with remains 28 feet. The 4-foot paved shoulder is proposed to be demarcated with a dashed white line, to separate pedestrians from vehicles.

Altus had previously provided justification for the similar design in a March 6, 2019 letter, citing concerns of control of vehicle speed, the fact that the roadway cannot be extended to connect to other roadways, and several other sources of low-volume roadway design standards that support the design in this case. Further, they described how the design is consistent with the general objectives of cluster subdivisions as expressed in the ordinance.

We believe the design is appropriate, and support granting the requested waiver. It is noted that the waiver assumes that the roadway remains private, as is proposed.

Should you have any questions, please do not hesitate to call.

Very truly yours,  
CMA ENGINEERS, INC.

William A. Straub, P.E.  
Project Manager

cc: Jeff Clifford, P.E. Altus Engineering

# TOWN OF KITTERY, MAINE

## TOWN PLANNING AND DEVELOPMENT DEPARTMENT

200 Rogers Road, Kittery, Maine 03904  
 PHONE: (207) 475-1323 FAX: (207) 439-6806  
[www.kittery.org](http://www.kittery.org)



### APPLICATION: MAJOR MODIFICATION TO AN APPROVED PLAN- SITE PLAN

<b>FEE FOR SITE PLAN AMENDMENT REVIEW: (TITLE 16.10.9.3)</b>	<input checked="" type="checkbox"/> <b>\$300.00 PLUS THE GREATER OF</b>	<input type="checkbox"/> <b>\$50/ADDITIONAL USE OF UNIT; OR</b>	<input type="checkbox"/> <b>\$5.00/100 SQ FT OF ADDITIONAL GROSS FLOOR AREA</b>	<b>Application Fee Paid:</b> \$ _____ Date: _____
		<input type="checkbox"/> <b>\$0.50/LINEAR FOOT OF ADDITIONAL DOCK, SLIP &amp; FLOAT; OR</b>	<input type="checkbox"/> <b>\$20.00/ADDITIONAL UNIT INTENDED TO PROVIDE OVERNIGHT SLEEPING ACCOMODATIONS</b>	<b>ASA Fee Paid:</b> (TITLE 3.3 TOWN CODE) \$ _____ Date: _____

<b>PROPERTY DESCRIPTION</b>	Parcel ID	Map	66	Lot	2A, 8 & 8A	Zone(s): Base:	R-RL <input checked="" type="checkbox"/> YES <input type="checkbox"/> NO	Total Land Area <del>(Square Feet)</del>	86.55 ac.
	Physical Address	40 Betty Welch Road							

<b>PROPERTY OWNER'S INFORMATION</b>	Name	Chinburg Development, LLC	<b>Mailing Address</b>	3 Penstock Way Newmarket, NH 03857	
	Phone	603-868-5995			
	Fax				
	Email	pkerrigan@chinburg.com			

<b>APPLICANT'S AGENT INFORMATION</b>	Name	same as owner	<b>Mailing Address</b>		
	Phone				
	Fax				
	Email				

<b>PROJECT DESCRIPTION</b>	Project Name: Huntington Run Subdivision
	Existing Use: Vacant wood lot that was recently harvested
	Proposed Amendment Please describe how the approved plan is proposed to be amended. State any known areas of non-compliance to the Town Code and how this amendment will decrease or remove non-compliance, if applicable.
	The applicant proposes to amendment the approved plan by providing a 4 foot wide paved shoulder on the south side of Huntington Way from its intersection with Betty Welch Road to the beginning of the cul-de-sac loop (adjacent to Lot 19). All other roadway shoulders are proposed as 1 foot of pavement with gravel shoulders of varying width as depicted the plans. The proposal requires a waiver from Section 16.8.4.4 and Table 1 Private Streets Standard – Street Width Design d. Paved Shoulder.
	I certify, to the best of my knowledge, this application information is true and correct and I will not deviate from the plan submitted without notifying the Kittery Town Planning Department of any changes.

Applicant's Signature: _____	Owner's Signature:
Date: _____	Date: 3-20-19

## Minimum Plan Submittal Requirements

- 15 COPIES OF THIS APPLICATION
- 15 COPIES OF THE APPROVED SITE PLAN – 12 REDUCED SIZE AT 11"X17"AND 3 FULL SIZE AT 24"X 36"
- 15 COPIES OF THE PROPOSED AMENDED SITE PLAN– 12 REDUCED SIZE AT 11"X17"AND 3 FULL SIZE AT 24"X 36"
- 1 PDF OF THE SITE PLAN SHOWING GPS COORDINATES

**PRIOR TO BEGINNING THE REVIEW PROCESS, THE PLANNING BOARD WILL DECIDE WHETHER SUFFICIENT INFORMATION HAS BEEN PROVIDED AND WILL VOTE TO *DETERMINE COMPLETENESS/ACCEPTANCE.***

**THE APPLICANT IS RESPONSIBLE TO PRESENT A CLEAR UNDERSTANDING OF THE PROPOSED AMENDMENT.**

- A) Paper size:  
 No less than 11" X 17" (reduced) or greater than 24" X 36" (full)
- B) Scale size:  
 Under 10 acres: no greater than 1" = 30'  
 10 + acres: 1" = 50'
- C) Title block:  
 Applicant's name and address  
 Name of preparer of plans with professional information and professional seal  
 Date of plan preparation  
 PARCEL'S TAX MAP ID (MAP/LOT) 1/4" TALL IN LOWER RIGHT  
 'SITE PLAN AMENDMENT' CLEARLY PART OF TITLE
- D) Clearly show how the approved plan will be amended.
- E) Provide signature blocks for amended approval.
- F) Provide all associated reference material and or documentation that clarifies and or supports the purpose of the proposed amendment.
- G) Revisions to the boundary, internal lots and or parcels must be signed and sealed by a surveyor licensed in the State of Maine.
- H) Revisions to the proposed site must be signed and sealed by a professional engineer licensed in the State of Maine.

**SEE TITLE 16.10.5.2 FOR COMPLETE LIST OF SUBMITTAL INFORMATION**

**NOTE TO APPLICANT: PRIOR TO THE SITE WALK, TEMPORARY MARKERS MUST BE ADEQUATELY PLACED THAT ENABLE THE PLANNING BOARD TO READILY LOCATE AND APPRAISE THE LAYOUT OF DEVELOPMENT.**

### Waiver Request

	Ordinance Section	Describe why this request is being made.
<b>DESCRIPTION</b>	***EXAMPLE*** 16.32.560 (B)-OFFSTREET PARKING.	***EXAMPLE*** Requesting a waiver of this ordinance since the proposed professional offices have a written agreement with the abutting Church owned property to share parking.
	16.8.4.4 and Table 1 Class III Private Street Stds.	Proposed 1' paved with 2' gravel shoulder on one side and 4' paved with 1' gravel shoulder on other side of 20' wide travelled way in lieu of the Minor Street stds.

**16.10.8.2.5 Conditions or Waivers.**

Conditions required by the Planning Board at the final plan review phase must have been met before the final plan may be given final approval unless so specified in the condition or specifically waived, upon written request by the applicant, by formal Planning Board action wherein the character and extent of such waivers which may have been requested are such that they may be waived without jeopardy to the public health, safety and general welfare.

**16.7.4.1 Objectives Met.** In granting modifications or waivers, the Planning Board must require such conditions as will, in its judgment, substantially meet the objectives of the requirements so waived or modified.

**SUBMITTALS THE TOWN PLANNER DEEMS SUFFICIENTLY LACKING IN CONTENT WILL NOT BE SCHEDULED FOR PLANNING BOARD REVIEW.**

**COMPLETED BY OFFICE STAFF**

ASA CHARGE	AMOUNT	ASA CHARGE	AMOUNT
<b>REVIEW</b>		<b>SERVICES</b>	
LEGAL FEES (TBD)		RECORDER	<b>\$35</b>
ENGINEERS REVIEW (TBD)		FACT FINDING (TBD)	
ABUTTER NOTICES		3 <sup>RD</sup> PARTY INSPECTIONS (TBD)	
POSTAGE	<b>\$20</b>	OTHER PROFESSIONAL SERVICES	<b>\$50</b>
LEGAL NOTICES		PERSONNEL	
ADVERTISING	<b>\$300</b>	SALARY CHARGES IN EXCESS OF 20 HOURS	
SUPPLIES			
OFFICE	<b>\$5</b>		
<b>SUB TOTAL</b>		<b>SUB TOTAL</b>	
		<b>TOTAL ASA REVIEW FEES</b>	





March 21, 2019

**Civil  
Site Planning  
Environmental  
Engineering**

133 Court Street  
Portsmouth, NH  
03801-4413

Jamie Steffen, Town Planner  
Town of Kittery  
200 Rogers Road  
Kittery, Maine 03904

Re: **Huntington Run Cluster Subdivision  
Map 66, Lots 2A, 8, and 8A  
40 Betty Welch Road  
Kittery, Maine**

Dear Mr. Steffen:

The Kittery Planning Board voted to approve the final plan for the Huntington Run Cluster Subdivision project at their March 14, 2019 meeting. On behalf of the applicant and owner, Chinburg Development, LLC, we respectfully submit the enclosed *Major Modification to an Approved Site Plan Application* to amend the approved plan by providing a 4 foot wide paved shoulder on the south side of Huntington Way from its intersection with Betty Welch Road to the beginning of the cul-de-sac loop (adjacent to Lot 19). All other roadway shoulders are proposed as 1 foot of pavement with gravel shoulders of varying width as depicted on the plans. The proposal requires a waiver from Section 16.8.4.4 and Table 1 Private Streets Standard – Street Width Design d. Paved Shoulder.

This submission includes the following materials:

- Application (15 copies)
- Approved Plan Sheets dated January 24, 2019: C-1.0 Lot Plan A, C-1.1 Lot Plan B, and C-6.0 Detail Sheet (3 sets 24"x36" and 12 sets 11"x17")
- Amended Plan Sheets dated March 21, 2019: C-1.0 Lot Plan A, C-1.1 Lot Plan B, and C-6.0 Detail Sheet (3 sets 24"x36" and 12 sets 11"x17")
- Exhibit 1 – Waiver Narrative, dated March 21, 2019 (15 copies)
- USGS Map (1 copy)
- Tax Map (1 copy)
- Abutters list and label (1 copy)
- Check for the Application Fees

The applicant seeks to be placed on the April 11, 2019 Planning Board meeting agenda. Please contact me if you have any questions or require additional information

Sincerely,

Jeffrey K. Clifford, P.E.  
Vice President

Enclosures

e-copies (w/encl.): Paul Kerrigan, Chinburg Development  
William Straub, PE, CMA

## EXHIBIT- A

### Huntington Run Cluster Subdivision

#### Waiver Request Narrative

March 21, 2019

At the March 14, 2019 Kittery Planning Board meeting, the applicant presented a waiver request from Section 16.8.4.4(d) of the Land Use and Development Code (LUDC) street standards. The proposal was to provide 1-foot wide paved shoulders in lieu of the 2-foot and 8-foot wide paved shoulders listed in Table 1, standards for Minor Streets, under *d. Paved Shoulder*. The Planning Board did not approve the waiver request as presented. In particular, Board members cited safety concerns about children traveling along Huntington Way to the school bus stop at Betty Welch Road. To address this concern, the applicant now proposes a 4-foot wide paved shoulder on the south side of Huntington Way from Betty Welch Road to the beginning of the cul-de-sac loop (adjacent to Lot 19), a distance of 935 feet. Striping will delineate the paved shoulder from the travelled way. With the 20-foot travelled way, the overall pavement width along this section of Huntington Way will be 25 feet. All other roadway shoulders are proposed to be constructed as 1-foot of pavement and gravel shoulders of varying width as depicted on the plans.

The proposed pavement now meets or exceeds the minimum width recommended by CMA in their review letter dated February 7, 2019. To the best of our knowledge, the section of 25 foot wide pavement width described above meets or exceeds the width of any subdivision roadway approved and constructed in Kittery during the last 20 years. Please note that early in the review process the applicant agreed to maintain a right-of-way width of 60-feet and a waiver from the sidewalk standard in Section 16.8.4.13 was approved by Board on April 13, 2017.

In support of this waiver request we are restating several points presented in the prior waiver request. At a meeting held with town staff on March 5, 2019, Altus provided information regarding roadway width and shoulders from the American Association of State Highway and Transportation Officials (AASHTO) publication titled "*Guidelines for Geometric Design of Very Low-Volume Local Roads (ADT ≤ 400)*". Attached is *Exhibit 2 - Guidelines for Total Roadway Width for New Construction of Urban Residential Streets* from the AASHTO publication. The developed portion of Huntington Run, at approximately four (4) dwelling units per acre, is considered a median development density by AASHTO for the purposes of *Exhibit 2*. Since the proposed houses will have garages and driveways providing sufficient off-street parking, the lower end of the range of total roadway width (28 feet) is applicable to Huntington Way. There was consensus at the meeting with staff that narrow pavement slows traffic and the proposed pavement and shoulders are appropriate for this project. It was recognized that Huntington Way will be predominately used by familiar drivers since there is no through traffic. The Chief of Police said cul-de-sac streets, where neighbors are known to each other, tend to be self-regulating in regards to vehicle speed. The DPW Commissioner said an overall 22-foot pavement width is appropriate for the 198 ADT at Huntington Run and noted that 22 feet is the pavement width listed in Table 1 of the LUDC for the

traveled way of a Secondary Collector Street. He also noted that the proposed 3-inch pavement thickness would conform to the LUDC requirement for a Secondary Collector (whereas the LUDC requirements for Minor Streets do not include a specific pavement thickness). CMA pointed out that the added pavement at the shoulder will help occasional large vehicles stay on pavement at the curves along the cul-de-sac. CMA recommended that 4-foot deep paved aprons be provided at each driveway. The applicant agreed, noting that they plan to fully pave the driveways. The applicant also agreed to post Huntington Way at 10 mph to promote safe travel conditions.

The LUDC's criteria for waiver authorization are presented in *Article IV. Waivers* as follows:

**16.7.4.1 Waiver Authorization.** (Ordained 9-26-11; Effective 10-27-11)

*Where the Planning Board finds, due to special circumstances of a particular plan, certain required improvements do not promote the interest of public health, safety and general welfare, or are inappropriate because of inadequacy or lack of connecting facilities adjacent or in proximity to the proposed development, upon written request, it may waive or modify such requirements, subject to appropriate conditions as determined by the Planning Board.*

For the Board's consideration, we offer the following responses to address the above criteria:

*"... due to special circumstances of a particular plan ..."*

**Response:** The proposed roadway section is reasonable in consideration of low traffic volume, low speed, and other successful clustered subdivision in Kittery with similar road widths. Huntington Way is designed as an extended cul-de-sac that serves only adjacent residences. There will never be through traffic to adjacent land. The predominate users of the road will be "familiar drivers". Vehicles entering Huntington Way from Betty Welch Road will be making a right angle turn and entering the street at low speed. With road curves and residences beginning within 450 feet of Betty Welch Road, drivers are expected to maintain a low speed. Striping will delineate the paved shoulder from the travelled way.

Huntington Way (with ADT 198) is characterized as a "Very Low-Volume Road in Rural Area" per the previously referenced AAHSTO publication. As noted above Huntington Way falls in the mid-range of very low-volume roads (ADT  $\leq$  400). Adequate on-site parking is provided with garages and driveways. The LUDC also does not differentiate subdivisions with adequate off-street parking whereas *Exhibit 2* addresses this important distinction and recognizes that there is no need for an 8-foot wide paved parking lane (shoulder). We also note that the LUDC standards do not differentiate between a subdivision with a single dead end road versus a subdivision of having a network of streets or through traffic.

*"... Certain required improvements do not promote the interest of public health, safety and general welfare..."*

**Response:** Roadways as well as sewer and water infrastructure are the types of "certain required improvements" that apply to subdivisions. The LUDC standard road section of 30 feet of pavement for a Minor Street would not serve to preserve the rural character of this section of Kittery. The attendees at the March 5<sup>th</sup> technical meeting acknowledged that narrow pavement width is an effective and appropriate traffic calming practice to lower vehicle speed. AASTHO identifies lower speed as the primary factor in

minimizing accidents. Narrower roads are also consistent with several objectives stated in the preamble of LUDC Section 16.8.11.1 - Cluster Development Purpose:

- A. efficient use of the land and water, with small networks of utilities and streets
- C. maintenance of rural character, preserving farmland, forest and rural views
- E. location of buildings and structures on those portions of the site most appropriate for development
- G. reduction of impacts on water resources by minimizing land disturbances and the creation of impervious surfaces and stormwater runoff

The Applicant agreed to a Planning Board member's suggested signage for pedestrians and bikes. Huntington Way will have a posted speed limit of 10 mph.

*"....or are inappropriate because of inadequacy or lack of connecting facilities adjacent or in proximity to the proposed development..."*

Betty Welch Road (pavement width of 23 feet, variable gravel shoulders, and posted at 35 mph) has neither sidewalks nor obvious travel destinations for pedestrians from Huntington Way.

Based on the above analysis, we respectfully request that the Planning Board approve the requested waiver relating to Section 16.8.4.4(d) Paved Shoulder.

**ATTACHMENT****Excerpt from: AASHTO - Guidelines for Geometric Design of Very Low-Volume Local Roads (ADT ≤ 400)****Very Low-Volume Local Roads in Urban Areas**

As in rural areas, the cross section width guidelines for very low-volume local roads in urban areas are related to basic operational requirements. Speeds are lower, trip lengths and lengths of local roads are generally much shorter, and available right-of-way width is much less than in rural areas. The major functional requirements for very low-volume local roads in urban areas include the ability for vehicles in opposite directions to pass one another, the need for vehicles to pass parked or stopped vehicles, the need to provide access for fire trucks and other emergency vehicles, and the need to accommodate occasional larger delivery vehicles.

Cross section widths for urban major access roads and urban industrial/commercial access roads should generally be the same as those shown for comparable rural roads in Exhibit 1. Greater widths are desirable where parking is permitted.

Cross section width guidelines for urban residential streets are shown in Exhibit 2. These widths incorporate consideration of access for fire trucks and other emergency vehicles and apply to streets where parking is permitted on both sides of the roadway. Reduced widths may be appropriate where parking is restricted. These guidelines are based on the ITE Recommended Guidelines for Subdivision Streets (13).

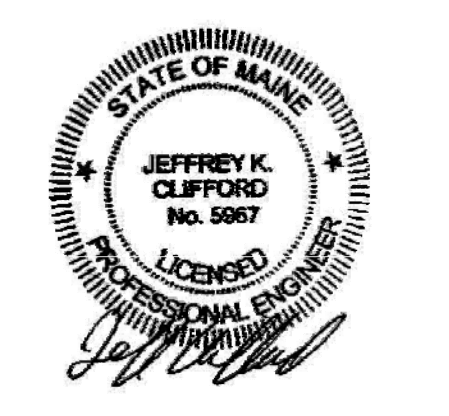
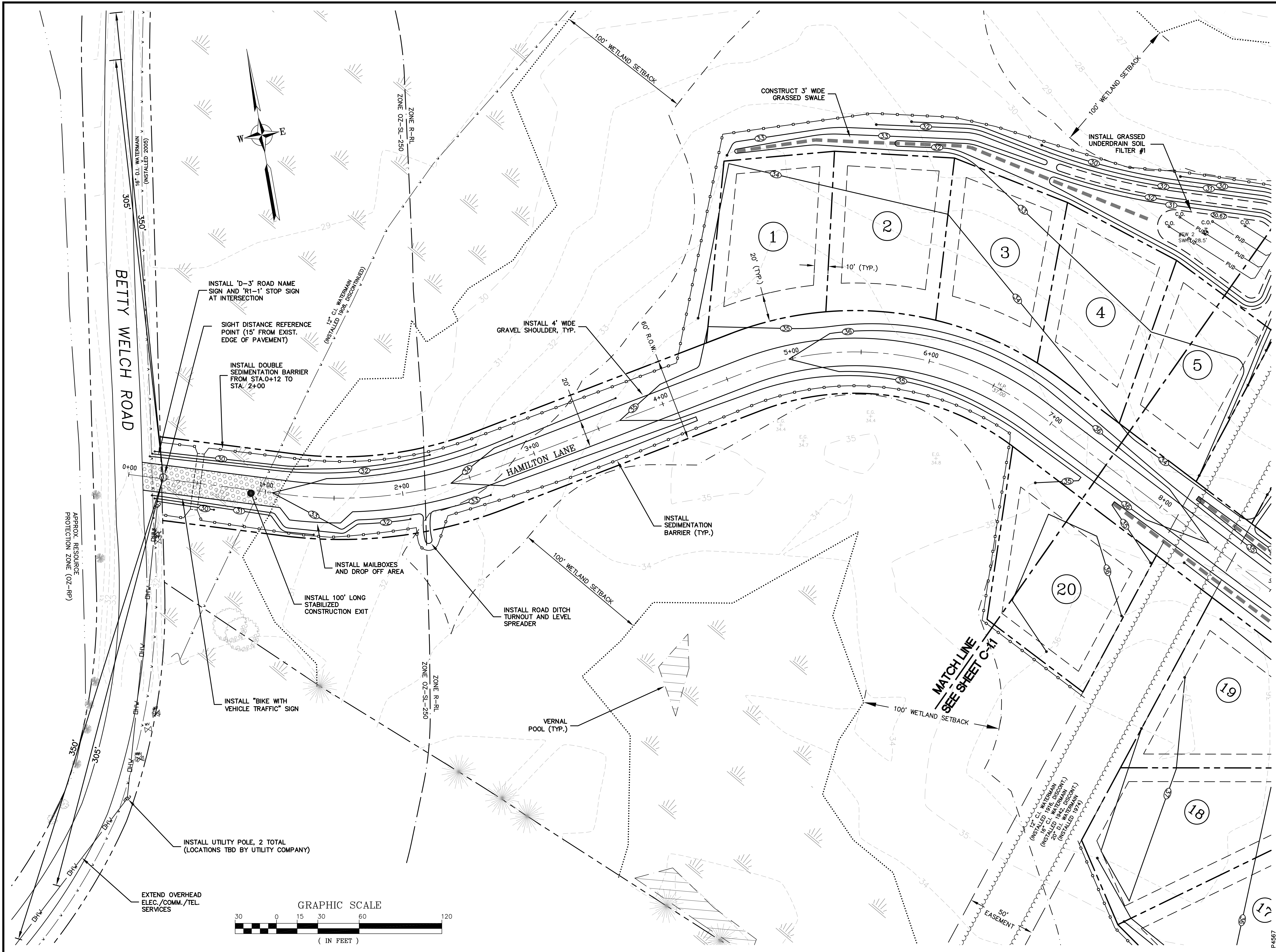
**Guidelines for Total Roadway Width for New Construction of Urban Residential Streets  
Exhibit 2., page 20, AASHTO ADT<400 2001**

Development density	US Customary Total roadway width (ft)
Low	20 to 28
Medium	28 to 34

Note: Low development density represents 2.0 or fewer dwelling units per acre; medium development density represents 2.1 to 6.0 dwelling units per acre.

The lower end of the range of residential street widths in the ITE guidelines presented in Exhibit 2 are applicable to subdivision streets with sufficient off-

street parking (e.g., driveways and garages) so that on-street parking is used only occasionally by visitors and delivery vehicles. The higher end of the range of street widths is applicable where there is frequent parking on one side of the street. On streets with frequent parking on both sides of the street, street widths greater than those shown in Exhibit 2 may be appropriate.



THIS DRAWING HAS NOT BEEN RELEASED FOR CONSTRUCTION

ISSUED FOR: FINAL APPROVAL

ISSUE DATE: JANUARY 24, 2019

NO.	DESCRIPTION	BY	DATE
0	PB SUBMISSION	JKC	6/22/17
1	MDEP SUBMISSION	JKC	3/19/18
2	REVISED ROADWAY WIDTH	JKC	8/23/18
3	TOWN FINAL APPROVAL	JKC	1/24/19

DRAWN BY: RMB  
APPROVED BY: JKC  
DRAWING FILE: 4567SITE.DWG

SCALE: 1" = 30'

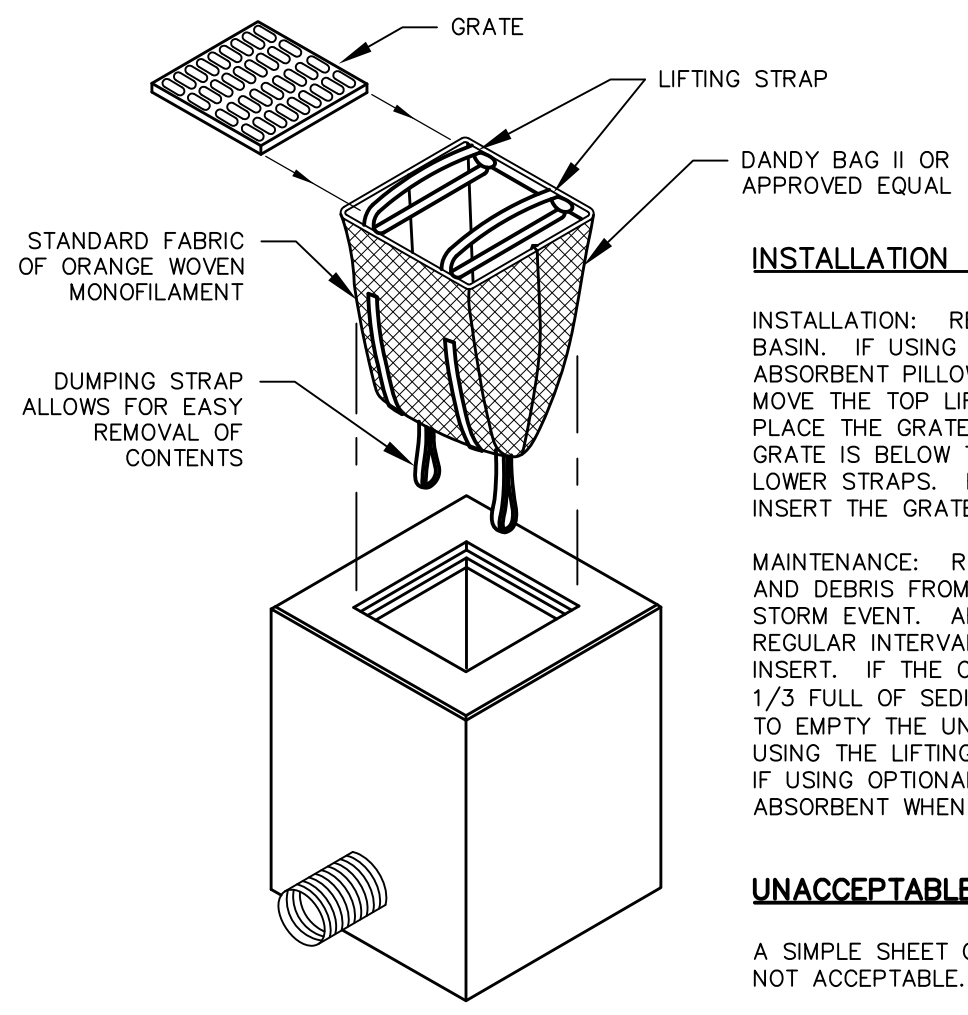
OWNERS/APPLICANT:  
**CHINBURG DEVELOPMENT, LLC**  
3 PENSTOCK WAY  
NEWMARKET, NH 03857

PROJECT:  
HUNTINGTON RUN  
SUBDIVISION  
MAP 66 LOTS 2A, 8 & 8A  
40 BETTY WELCH ROAD  
KITTERY, MAINE

TITLE:  
**LOT PLAN - A**  
SHEET NUMBER:  
**C-10**







**INSTALLATION AND MAINTENANCE:**

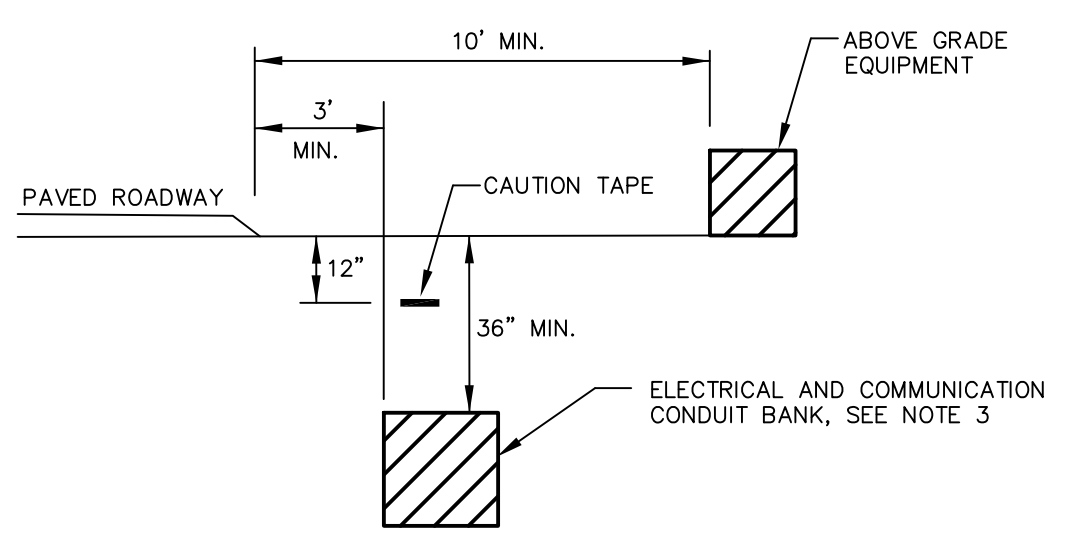
**INSTALLATION:** REMOVE THE GRATE FROM CATCH BASIN. IF USING OPTIONAL OIL ABSORBENTS, PLACE ABSORBENT PILLOW IN UNIT. STAND GRATE ON END. MOVE THE TOP LIFTING STRAPS OUT OF THE WAY AND PLACE THE GRATE INTO CATCH BASIN. INSERT SO THE GRATE IS BELOW THE TOP STRAPS AND ABOVE THE LOWER STRAPS. HOLDING THE LIFTING DEVICES, INSERT THE GRATE INTO THE INLET.

**MAINTENANCE:** REMOVE ALL ACCUMULATED SEDIMENT AND DEBRIS FROM VICINITY OF THE UNIT AFTER EACH STORM EVENT. AFTER EACH STORM EVENT AND AT REGULAR INTERVALS, LOOK INTO THE CATCH BASIN. INSERT. IF THE CONTAINMENT AREA IS MORE THAN 1/3 FULL OF SEDIMENT, THE UNIT MUST BE EMPTIED. TO EMPTY THE UNIT, LIFT THE UNIT OUT OF THE INLET USING THE LIFTING STRAPS AND REMOVE THE GRATE. IF USING OPTIONAL ABSORBENTS, REPLACE ABSORBENT WHEN NEAR SATURATION.

**UNACCEPTABLE INLET PROTECTION METHOD:**

A SIMPLE SHEET OF GEOTEXTILE UNDER THE GRATE IS NOT ACCEPTABLE.

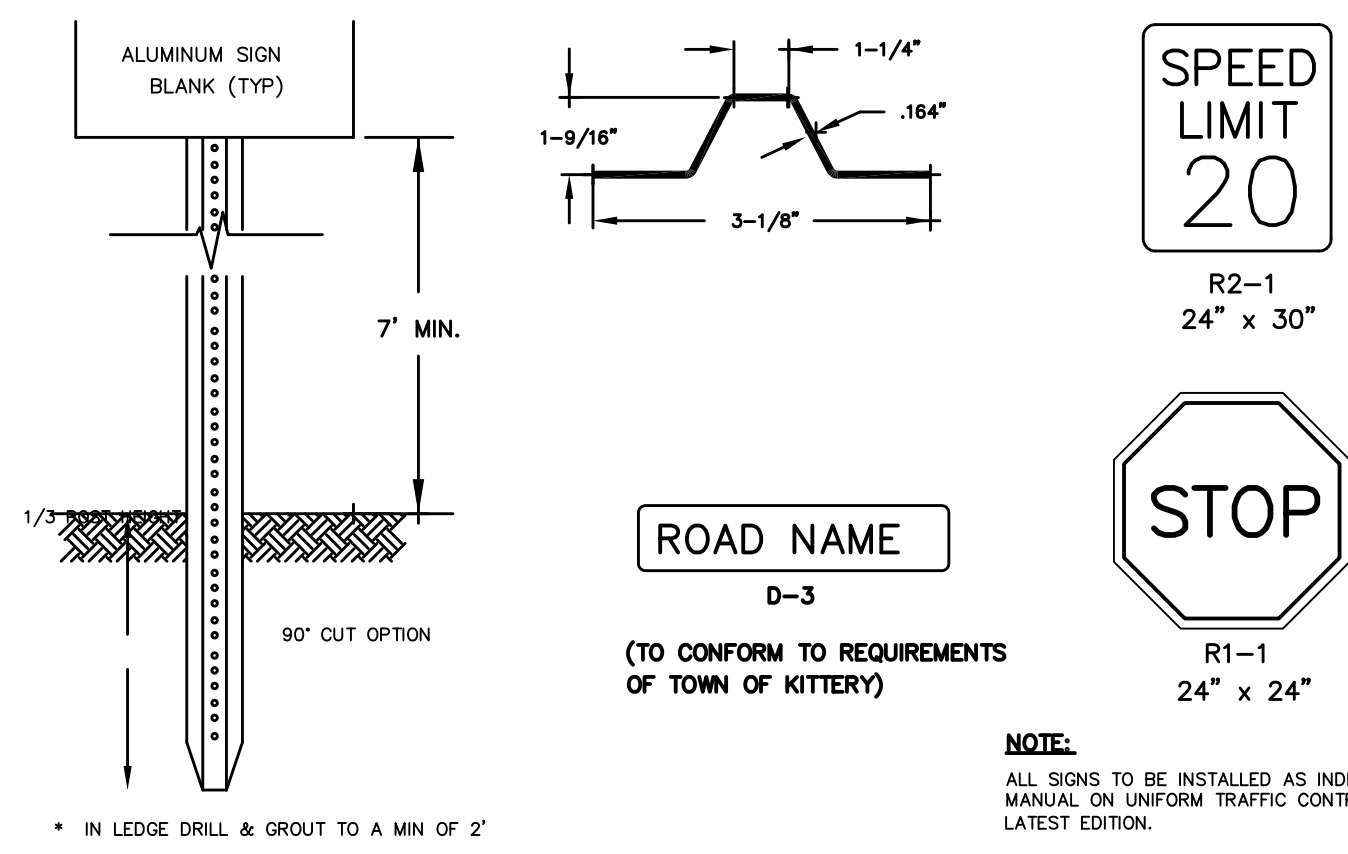
**STORM DRAIN INLET PROTECTION**  
NOT TO SCALE



**NOTES:**

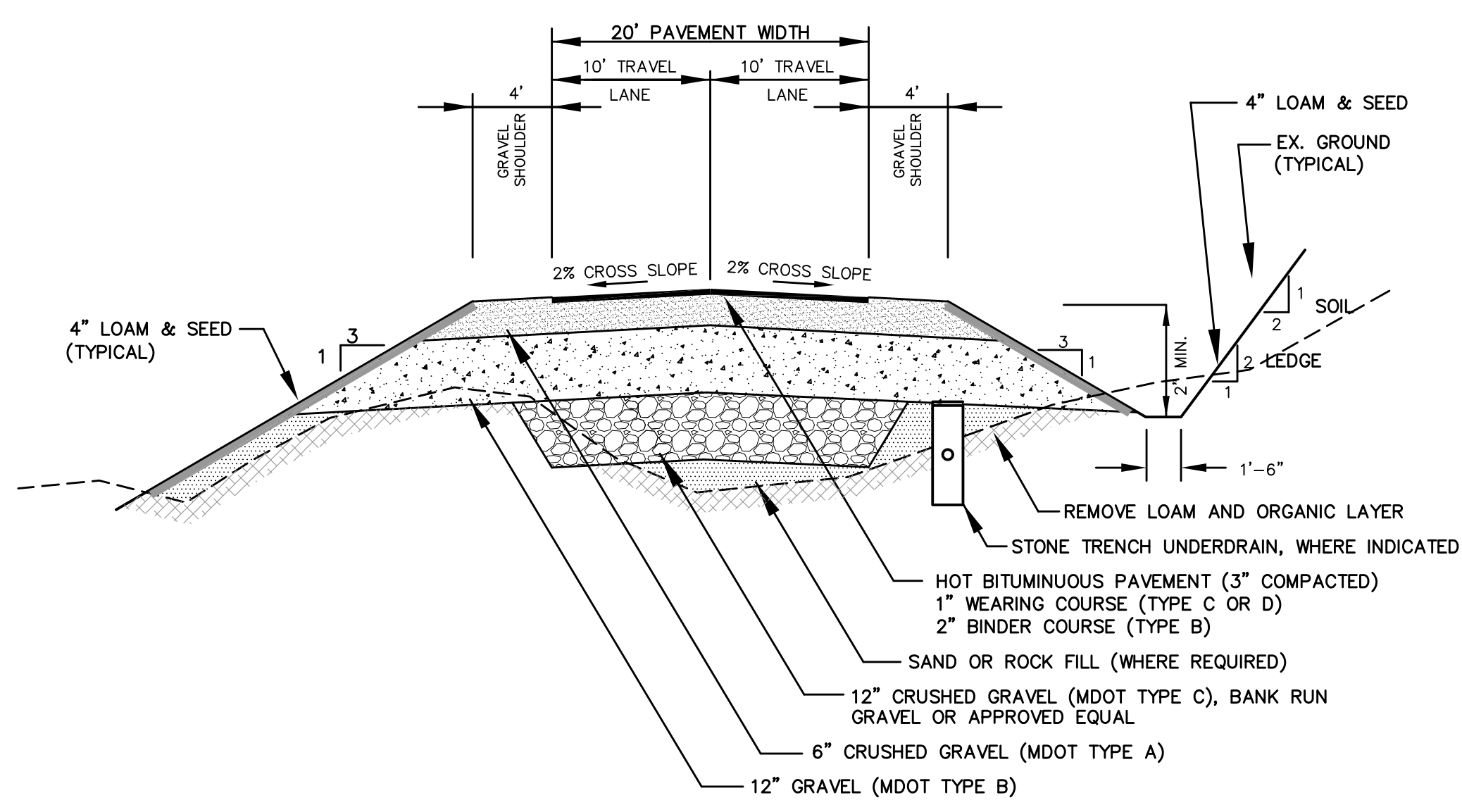
- ELECTRICAL AND COMMUNICATION CONDUIT SIZE, MATERIALS AND SPACING SHALL BE IN ACCORDANCE WITH THE APPLICABLE UTILITY COMPANY STANDARDS.
- CONDUIT SHALL CROSS PAVED AREAS AT 90°.
- BACKFILL NOTES:
  - SELECTED SAND BACKFILL SHALL CONSIST OF A FINE GRANULAR MATERIAL OF WHICH 100% SHALL PASS THROUGH A 1/4" SIEVE.
  - EXCEPTION: NATURALLY OCCURRING SMOOTH ROUND PEBBLES NO GREATER THAN 3/8" IN DIAMETER ARE PERMITTED AS LONG AS THEIR TOTAL VOLUME PER CUBIC FOOT OF SAND DOES NOT EXCEED 1%.
  - THE SAND SHALL BE COMPLETELY FREE OF FROZEN LUMPS, ROCKS, STONES, DEBRIS AND RUBBISH.

**UNDERGROUND CONDUIT BANK DETAIL**  
NOT TO SCALE

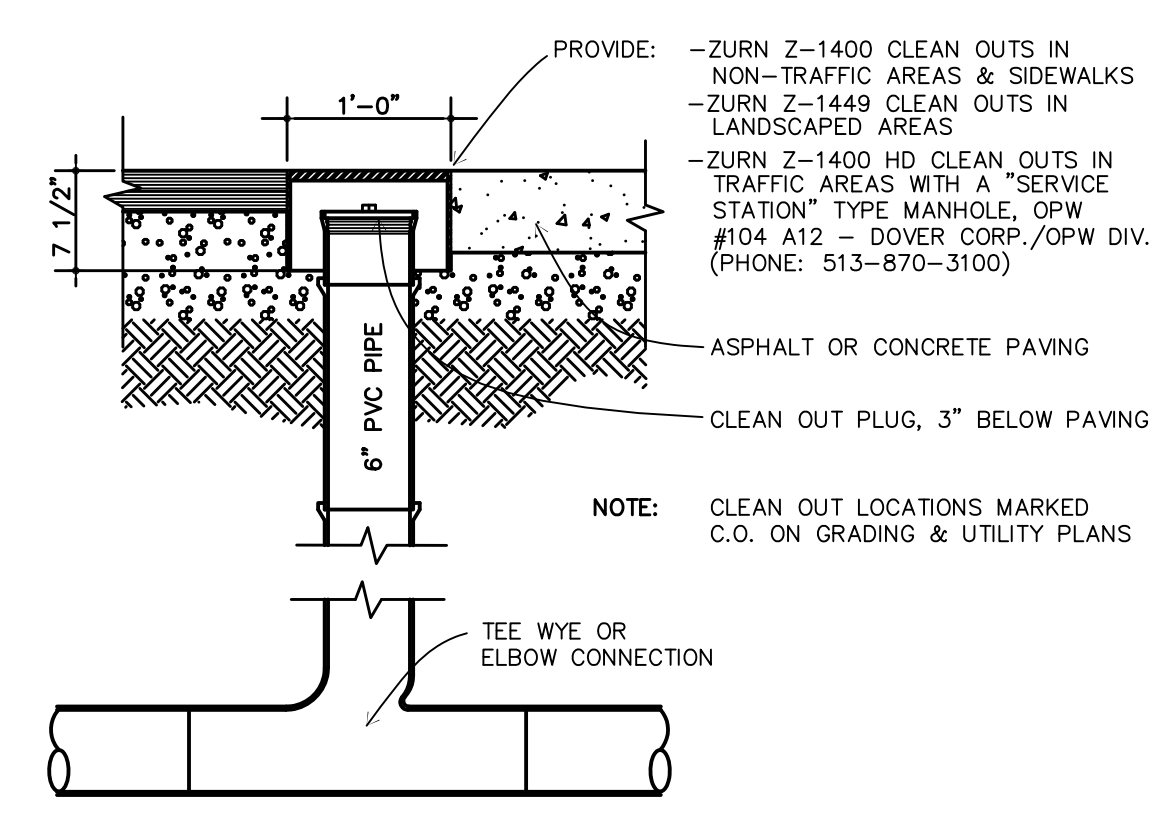


LENGTH: AS REQUIRED  
WEIGHT PER LINEAR FOOT: 2.50 LBS (MIN.)  
HOLES: 3/8" DIAMETER, 1" C-C FULL LENGTH  
STEEL: SHALL CONFORM TO ASTM A-499 (GRADE 60) OR ASTM A-576 (GRADE 1070 - 1080)  
FINISH: SHALL BE PAINTED WITH TWO COATS OF AN APPROVED MEDIUM GREEN BAKED ON OR AIR DRIED, PAINT OF WEATHER RESISTANT QUALITY. ALL FABRICATION SHALL BE COMPLETE BEFORE PAINTING.

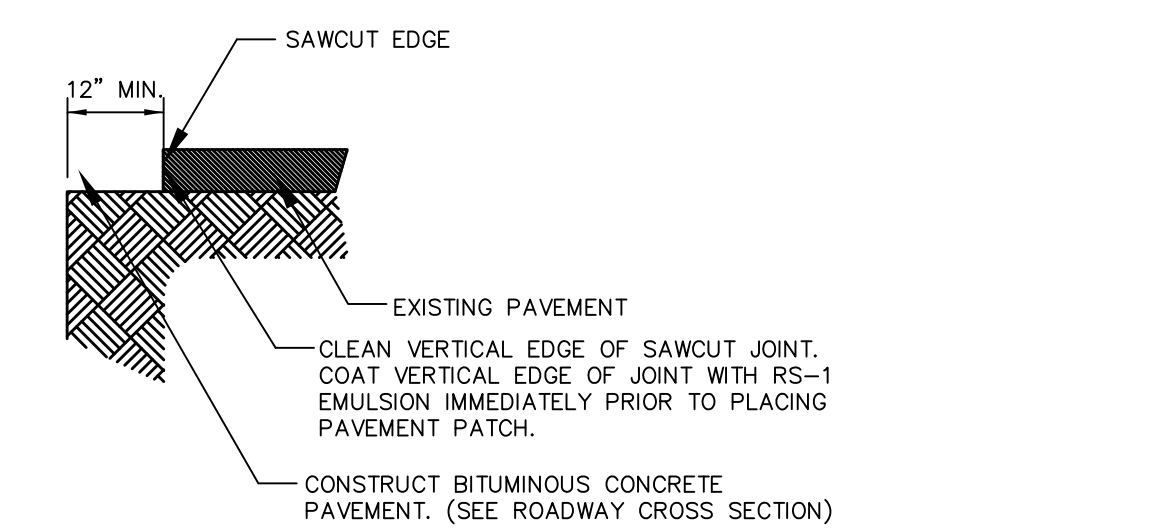
**SIGN POST DETAIL**  
NOT TO SCALE



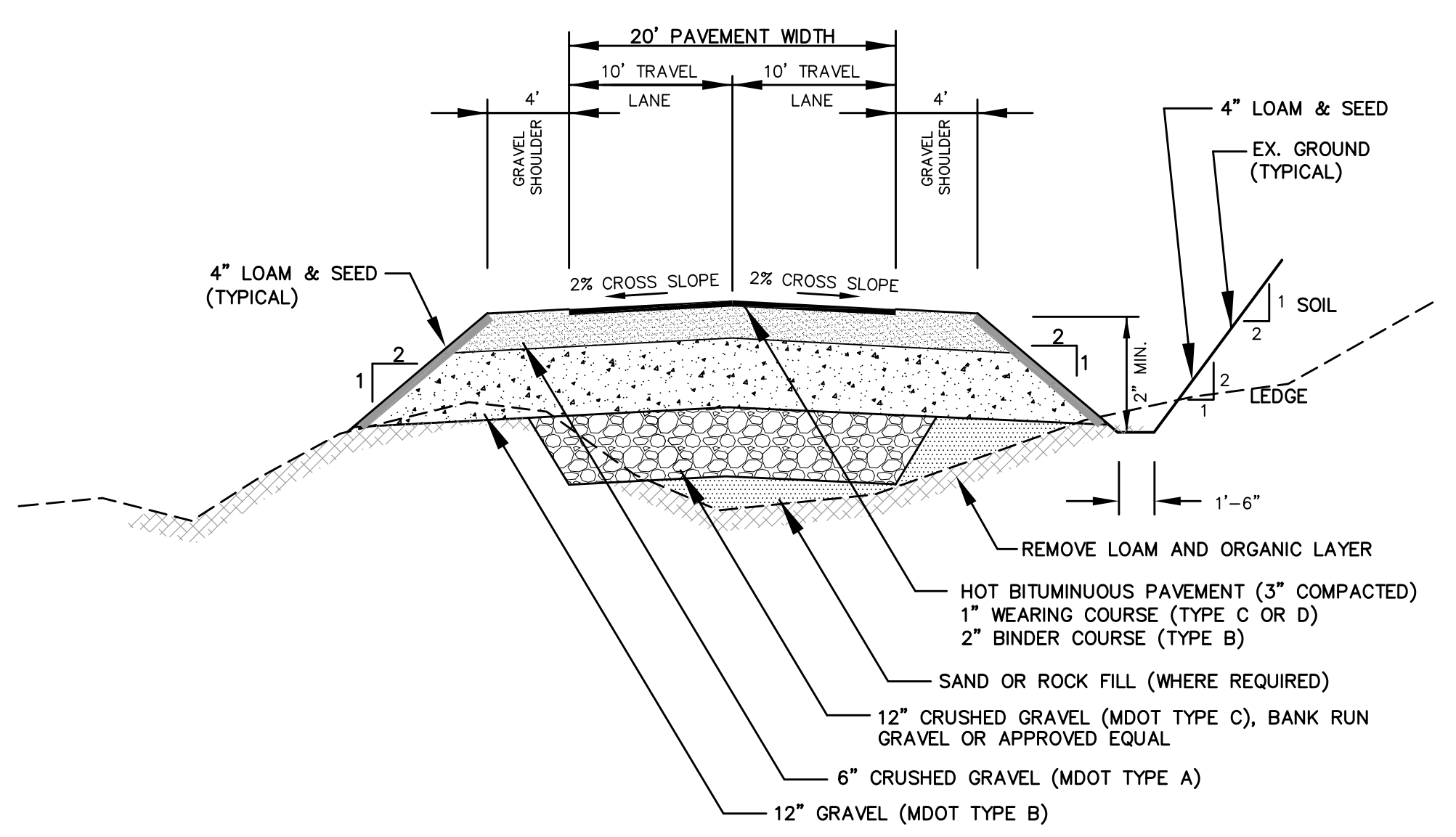
**TYPICAL ROADWAY CROSS SECTION**  
STA. 1+10 TO STA. 16+39



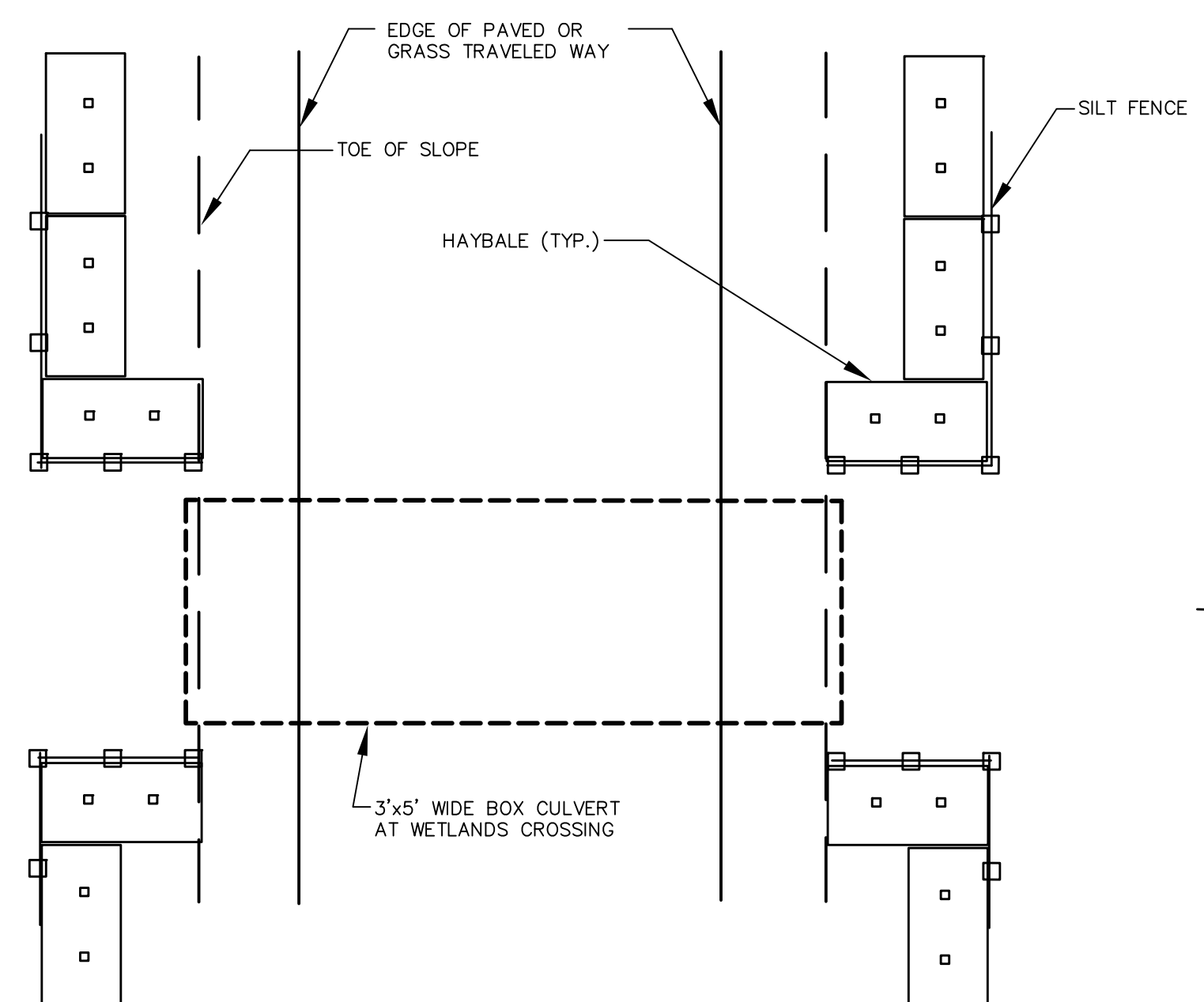
**CLEANOUT DETAIL**  
NOT TO SCALE



**TYPICAL PAVEMENT SAWCUT DETAIL**  
NOT TO SCALE

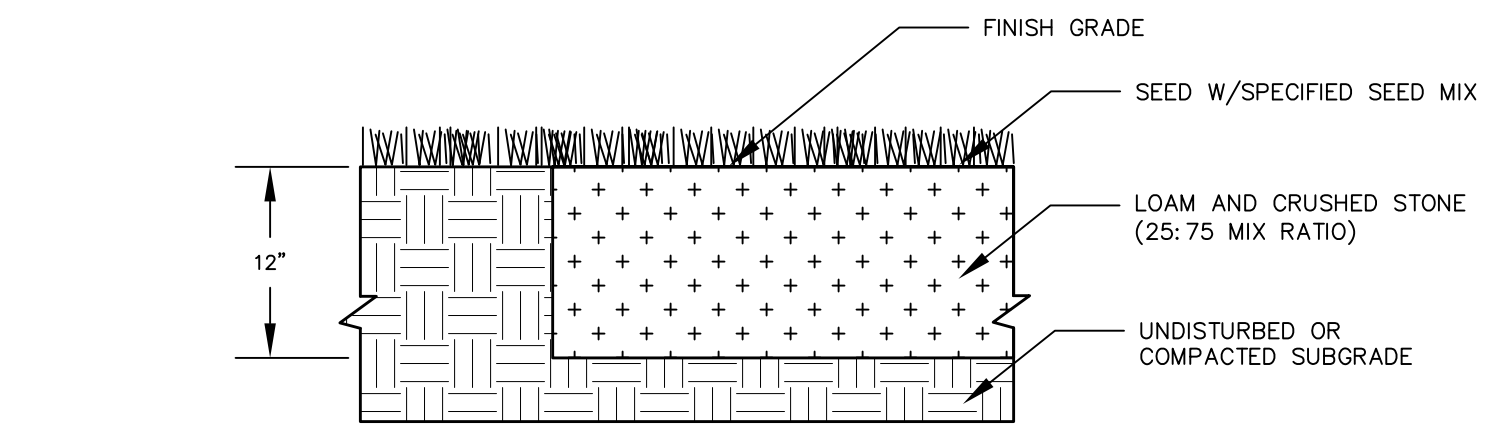


**TYPICAL ROADWAY CROSS SECTION (WETLANDS CROSSING)**  
STA. 0+10 TO STA. 1+10  
NOT TO SCALE

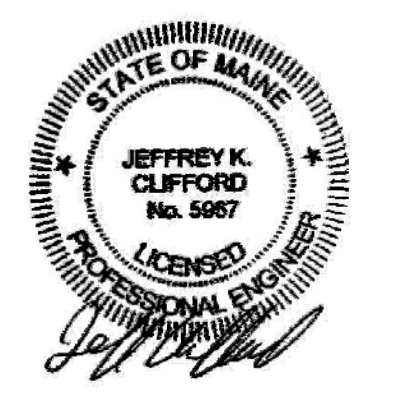
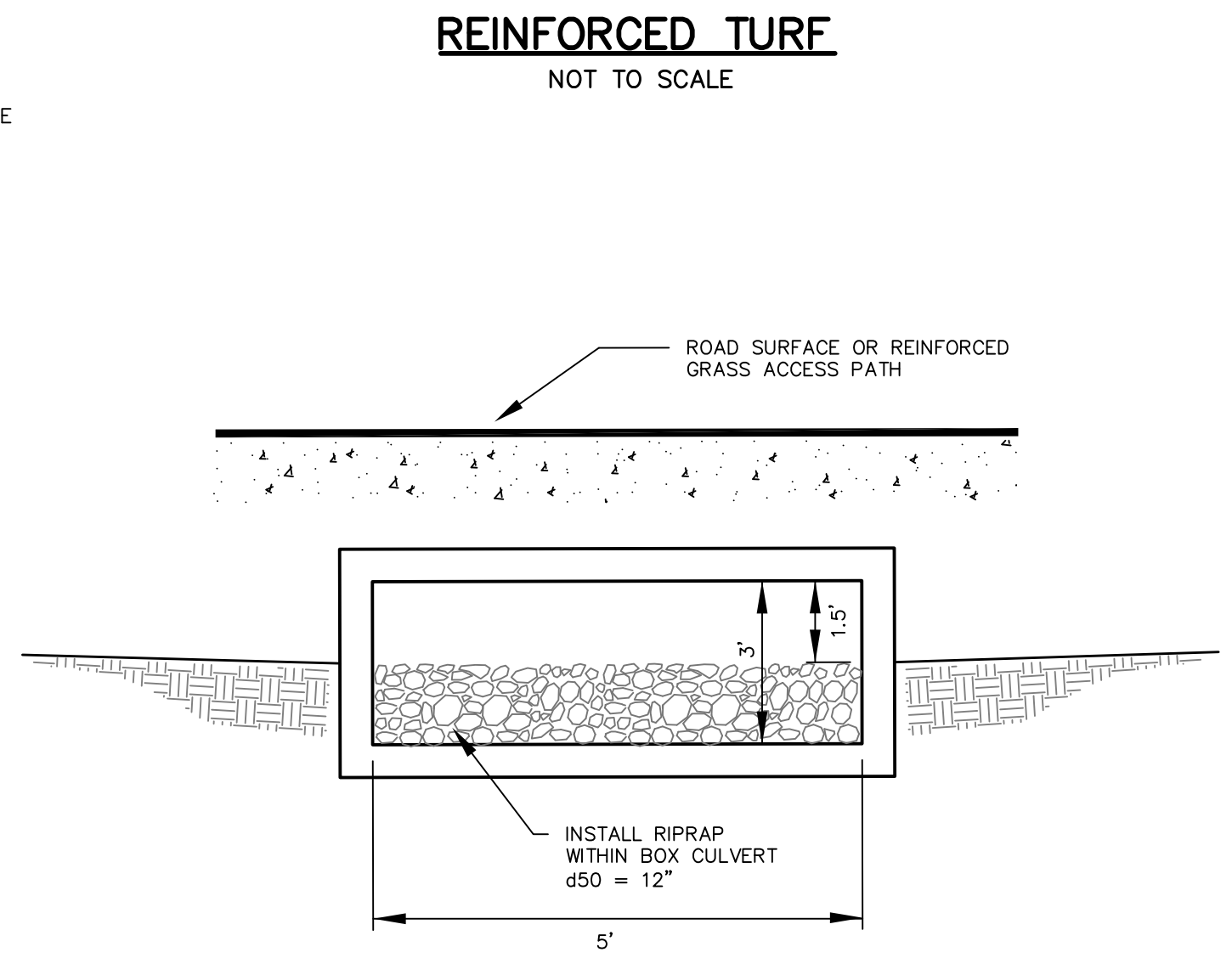


**PLAN**

**CULVERT DETAIL AT WETLANDS X-ING**  
NOT TO SCALE



**REINFORCED TURF**  
NOT TO SCALE



**THIS DRAWING HAS NOT BEEN RELEASED FOR CONSTRUCTION**

ISSUED FOR: **FINAL APPROVAL**

ISSUE DATE: **JANUARY 24, 2019**

NO.	DESCRIPTION	BY	DATE
0	PB SUBMISSION	JKC	6/22/17
1	MDEP SUBMISSION	JKC	3/19/18
2	REVISED ROADWAY X-SEC	JKC	8/23/18
3	TOWN FINAL APPROVAL	JKC	1/24/19

DRAWN BY: **RMB**  
APPROVED BY: **JKC**  
DRAWING FILE: **4567SITE.DWG**

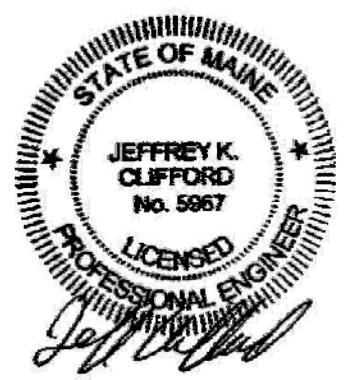
SCALE: **N.T.S.**

OWNER/APPLICANT:  
**CHINBURG DEVELOPMENT, LLC**  
3 PENSTOCK WAY  
NEWMARKET, NH 03857

PROJECT:  
**HUNTINGTON RUN SUBDIVISION**  
MAP 66 LOTS 2A, 8 & 8A  
40 BETTY WELCH ROAD  
KITTERY, MAINE

TITLE:  
**DETAIL SHEET**

SHEET NUMBER:  
**C-6.0**



**THIS DRAWING HAS NOT BEEN RELEASED FOR CONSTRUCTION**

ISSUED FOR:

**MODIFICATION OF APPROVAL**

ISSUE DATE:

**MARCH 21, 2019**

NO.	DESCRIPTION	BY	DATE
0	PB SUBMISSION	JKC	6/22/17
1	MDEP SUBMISSION	JKC	3/19/18
2	REVISED ROADWAY WIDTH	JKC	8/23/18
3	TOWN FINAL APPROVAL	JKC	1/24/19
4	ADD WALKWAY	JKC	3/21/19

DRAWN BY: \_\_\_\_\_ RMB  
APPROVED BY: \_\_\_\_\_ JKC  
DRAWING FILE: 4567SITE.DWG

SCALE: 1" = 30'

OWNERS/APPLICANT:

**CHINBURG DEVELOPMENT, LLC**  
3 PENSTOCK WAY  
NEWMARKET, NH 03857

PROJECT:

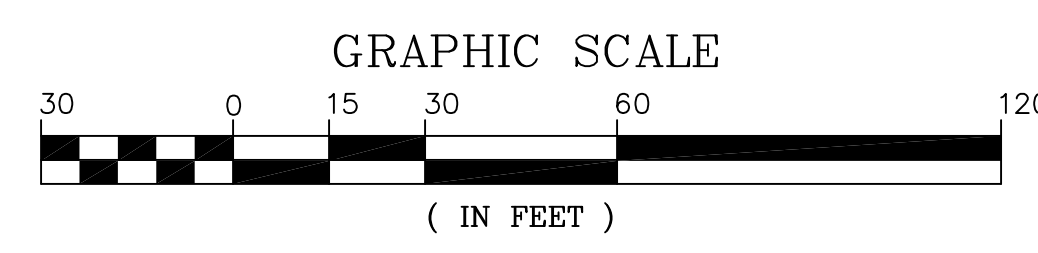
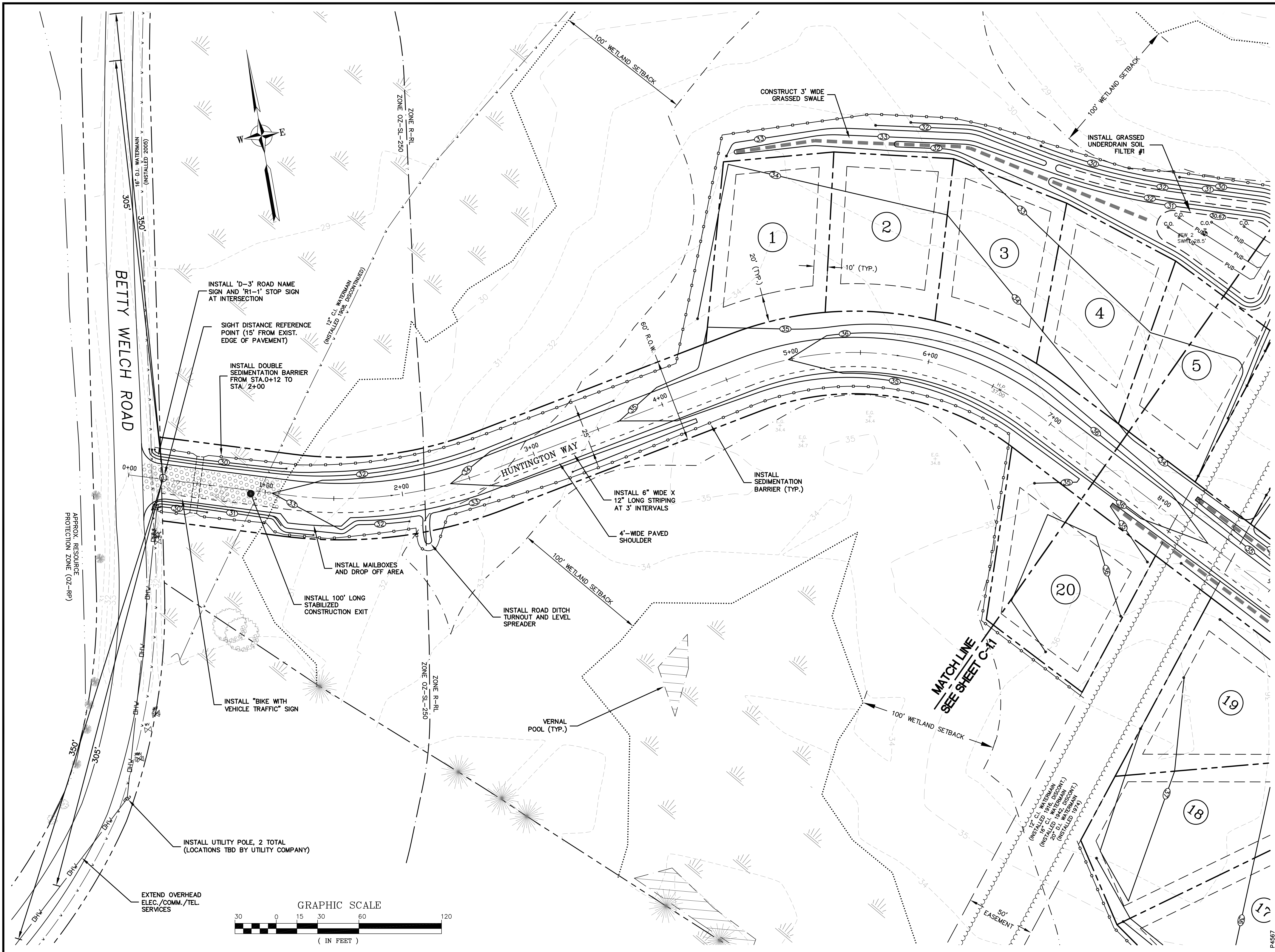
**HUNTINGTON RUN SUBDIVISION**  
MAP 66 LOTS 2A, 8 & 8A  
40 BETTY WELCH ROAD  
KITTERY, MAINE

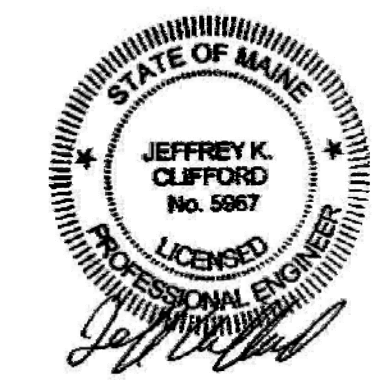
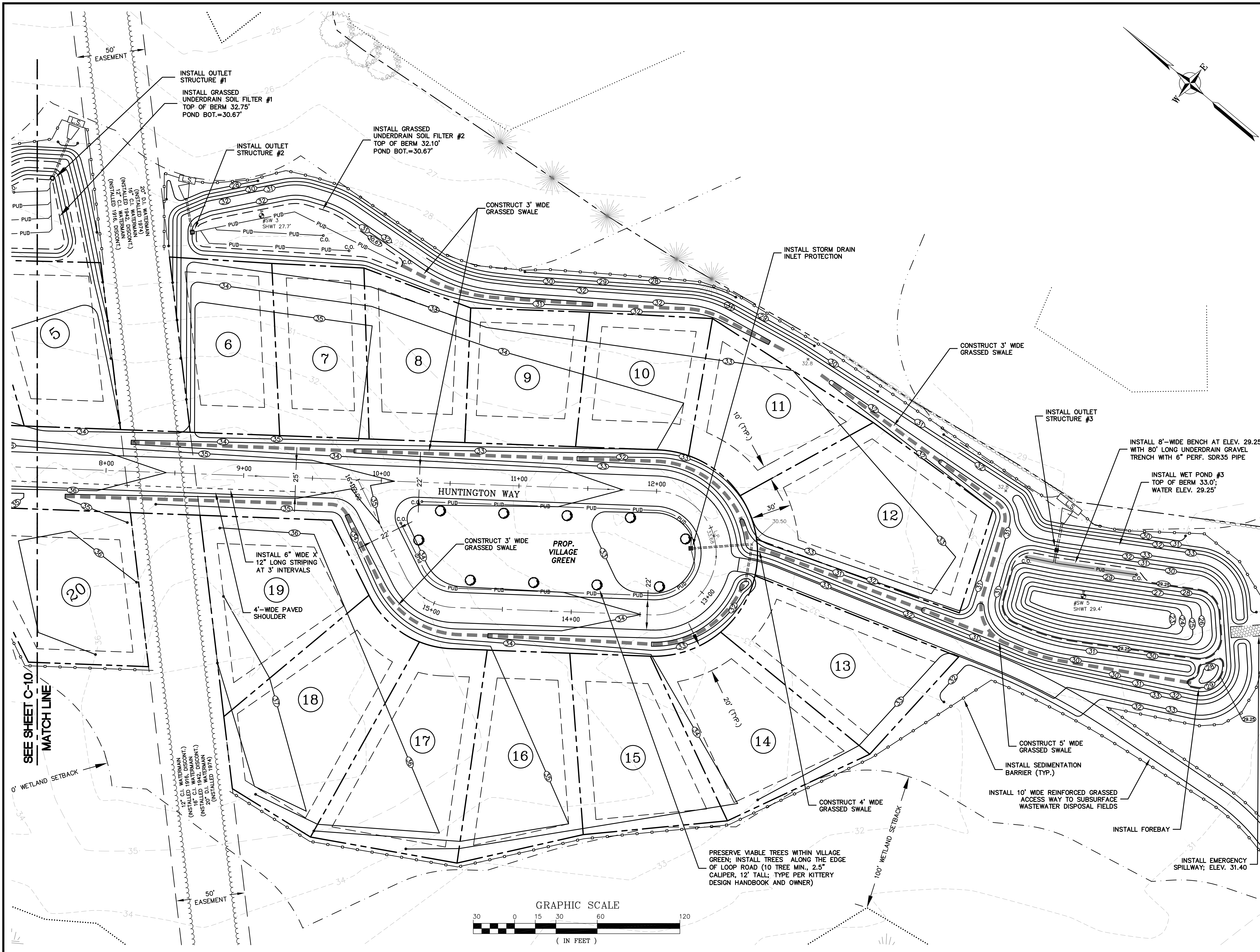
TITLE:

**LOT PLAN - A AMENDMENT**

SHEET NUMBER:

**C-10**





**THIS DRAWING HAS NOT BEEN RELEASED FOR CONSTRUCTION**

ISSUED FOR:  
**MODIFICATION OF APPROVAL**

ISSUE DATE:  
**MARCH 21, 2019**

NO.	DESCRIPTION	BY	DATE
0	PB SUBMISSION	JKC	6/22/17
1	MDEP SUBMISSION	JKC	3/19/18
2	MDEP COMMENTS	JKC	7/13/18
3	REVISED ROADWAY WIDTH	JKC	8/29/18
4	MDEP COMMENTS	JKC	9/10/18
5	TOWN FINAL APPROVAL	JKC	1/24/19
6	ADD WALKWAY	JKC	3/21/19

DRAWN BY: RMB  
APPROVED BY: JKC  
DRAWING FILE: 4567SITE.DWG

SCALE: 1" = 30'

OWNERS/APPLICANT:  
**CHINBURG DEVELOPMENT, LLC**  
3 PENSTOCK WAY  
NEWMARKET, NH 03857

PROJECT:

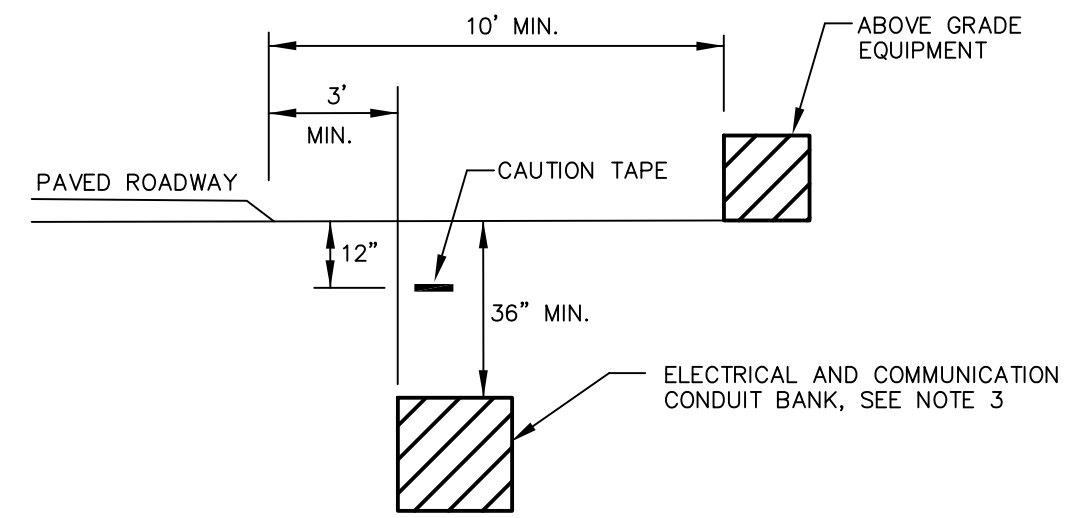
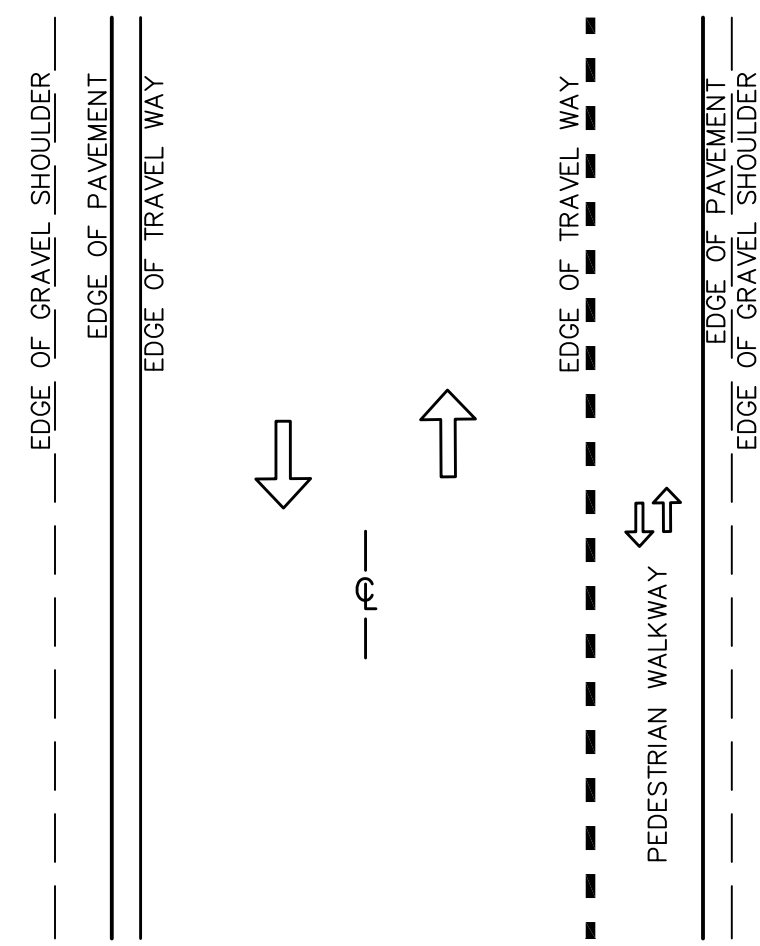
TITLE:

**LOT PLAN - B AMENDMENT**

SHEET NUMBER:

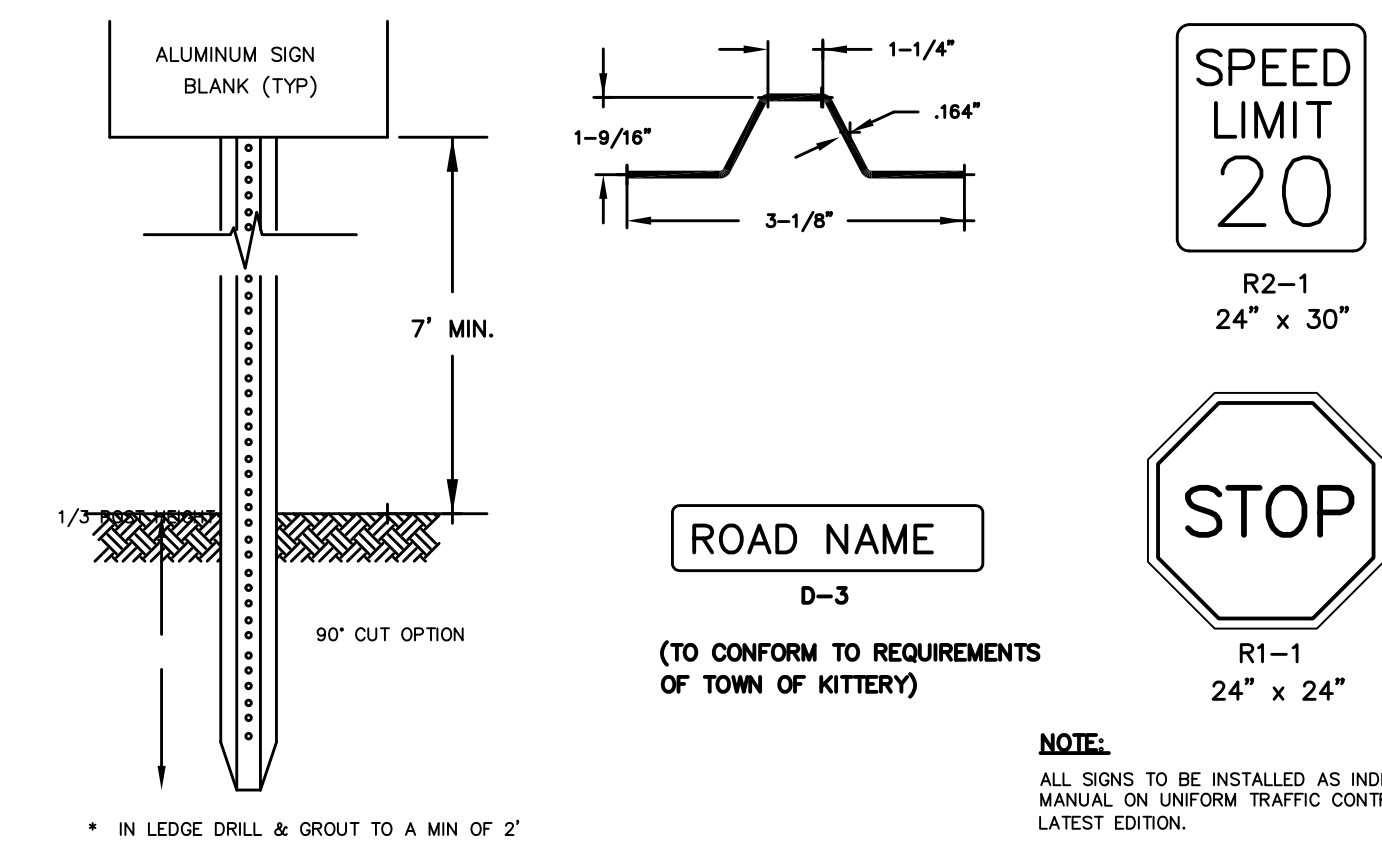
**C-11**

P4567



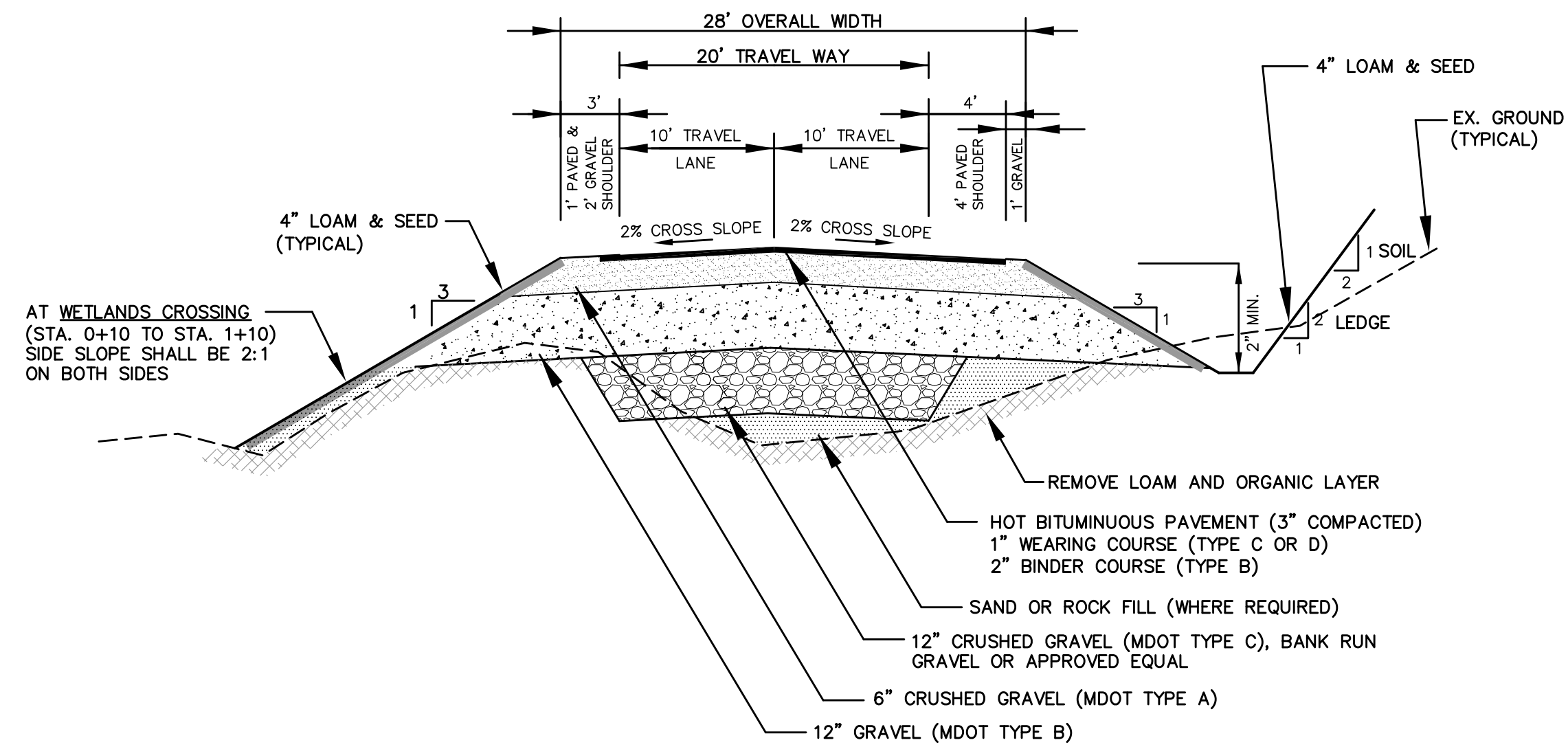
- NOTES:**
- ELECTRICAL AND COMMUNICATION CONDUIT SIZE, MATERIALS AND SPACING SHALL BE IN ACCORDANCE WITH THE APPLICABLE UTILITY COMPANY STANDARDS.
  - CONDUIT SHALL CROSS PAVED AREAS AT 90°.
  - BACKFILL NOTES:
    - SELECTED SAND BACKFILL SHALL CONSIST OF A FINE GRANULAR MATERIAL OF WHICH 100% SHALL PASS THROUGH A 1/4" SIEVE.
    - EXCEPTION: NATURALLY OCCURRING SMOOTH ROUND PEBBLES NO GREATER THAN 3/8" IN DIAMETER ARE PERMITTED AS LONG AS THEIR TOTAL VOLUME PER CUBIC FOOT OF SAND DOES NOT EXCEED 1%.
    - THE SAND SHALL BE COMPLETELY FREE OF FROZEN LUMPS, ROCKS, STONES, DEBRIS AND RUBBISH.

**UNDERGROUND CONDUIT BANK DETAIL**  
NOT TO SCALE

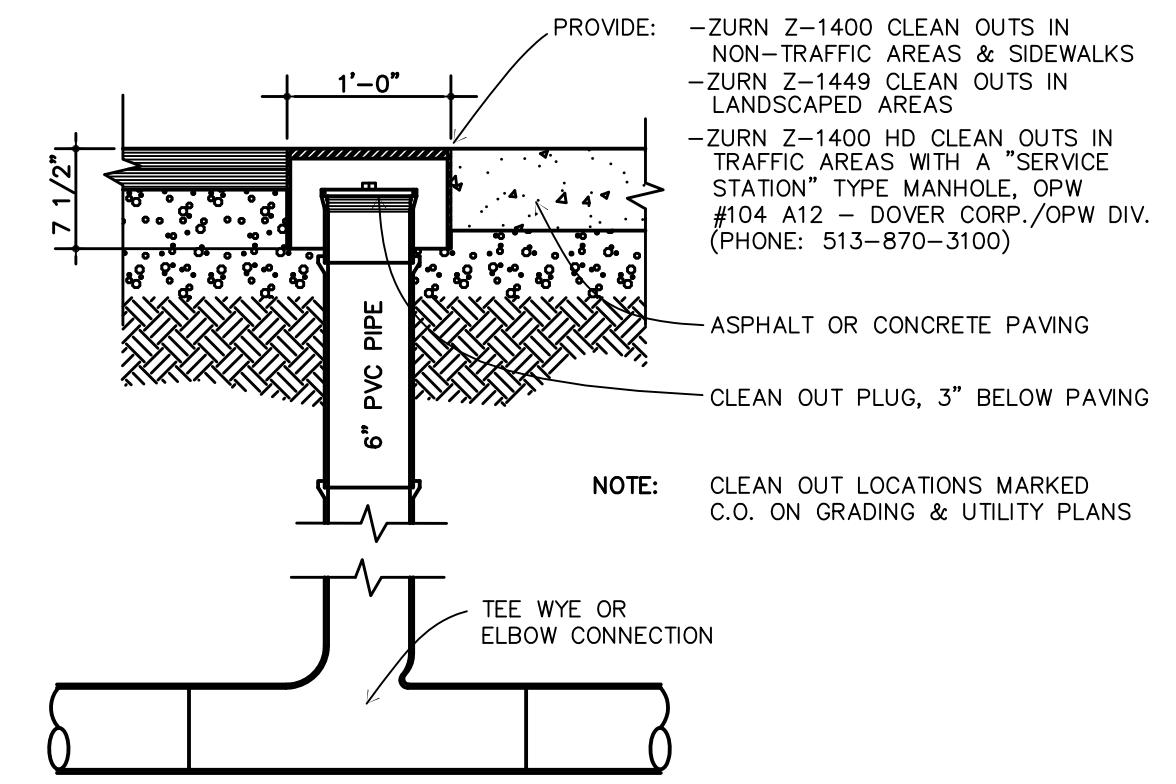


\* IN LEDGE DRILL & GROUT TO A MIN OF 2'  
 LENGTH: AS REQUIRED  
 WEIGHT PER LINEAR FOOT: 2.50 LBS (MIN.)  
 HOLES: 3/8" DIAMETER, 1" C-C FULL LENGTH  
 STEEL: SHALL CONFORM TO ASTM A-499 (GRADE 60) OR ASTM A-576 (GRADE 1070 - 1080)  
 FINISH: SHALL BE PAINTED WITH TWO COATS OF AN APPROVED MEDIUM GREEN BAKED ON OR AIR DRIED, PAINT OF WEATHER RESISTANT QUALITY. ALL FABRICATION SHALL BE COMPLETE BEFORE PAINTING.

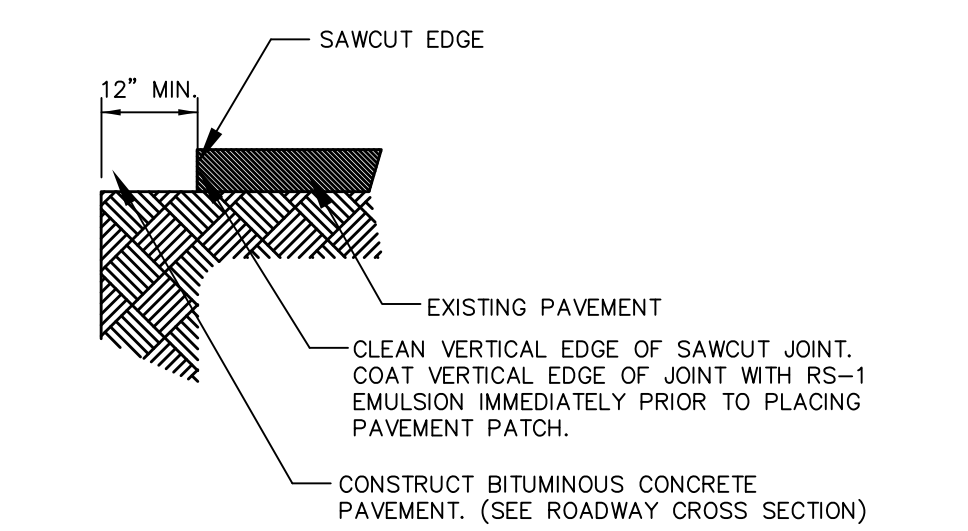
**SIGN POST DETAIL**  
NOT TO SCALE



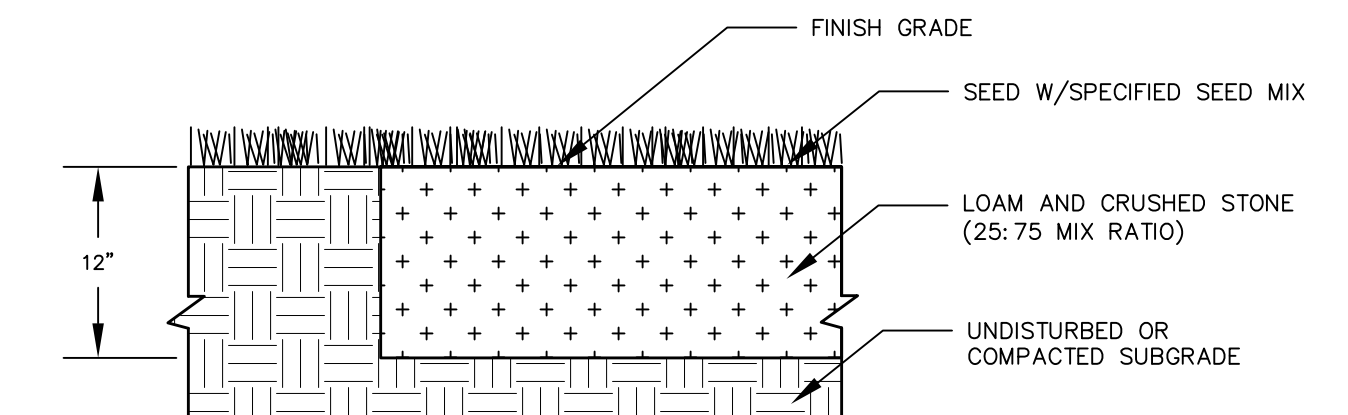
**TYPICAL ROADWAY CROSS SECTION**  
STA. 0+10 TO STA. 9+80  
NOT TO SCALE



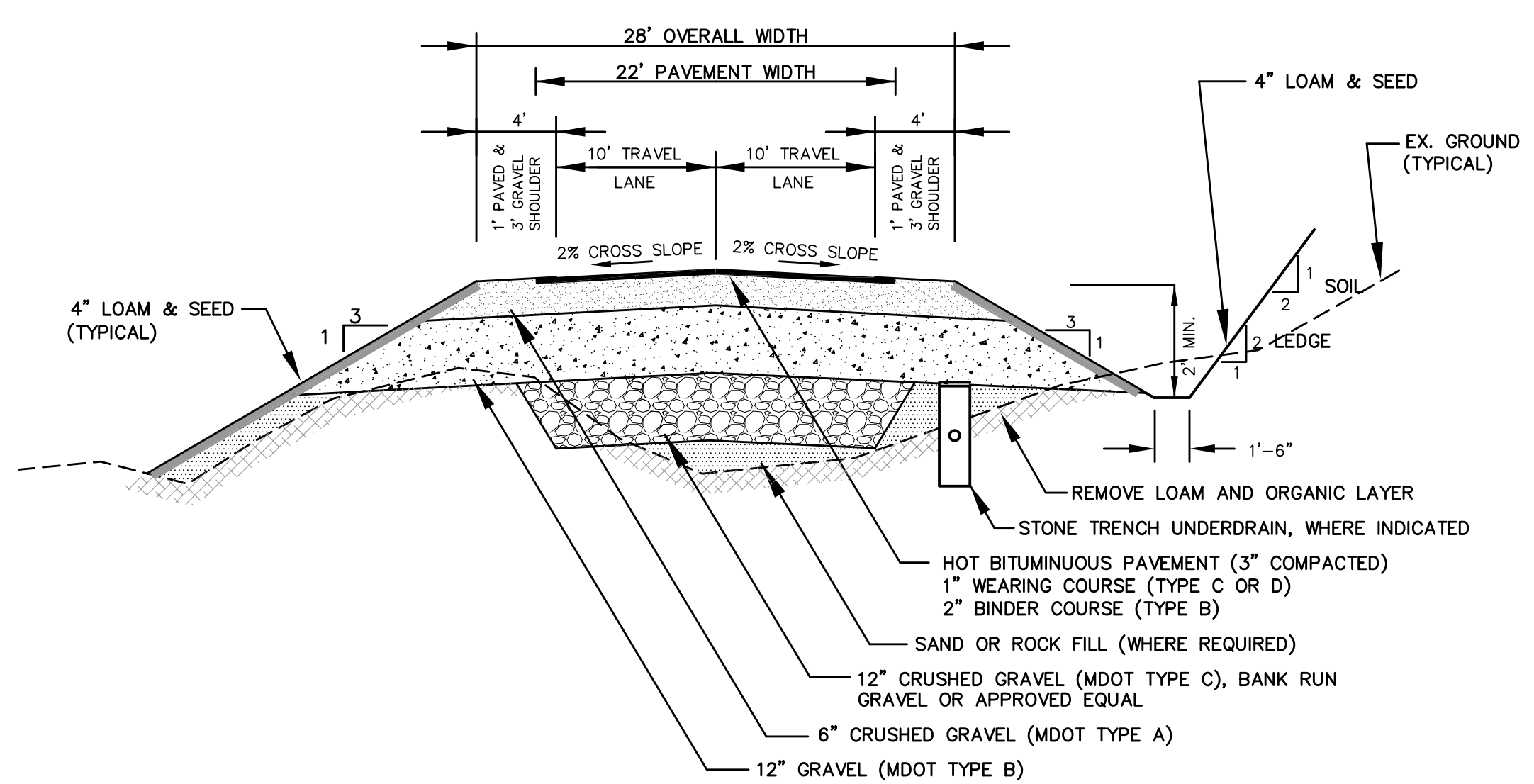
**CLEANOUT DETAIL**  
NOT TO SCALE



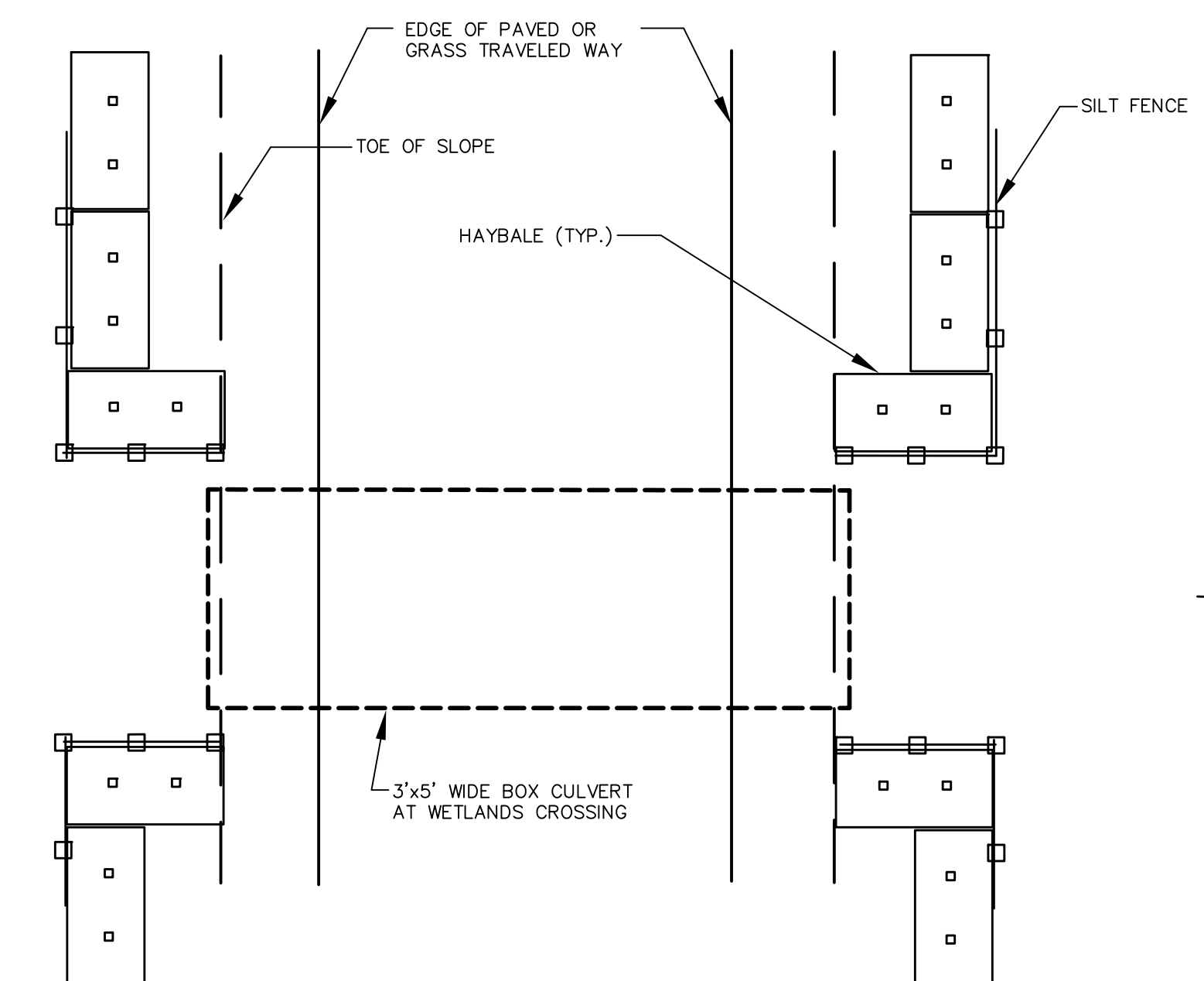
**TYPICAL PAVEMENT SAWCUT DETAIL**  
NOT TO SCALE



**REINFORCED TURF**  
NOT TO SCALE

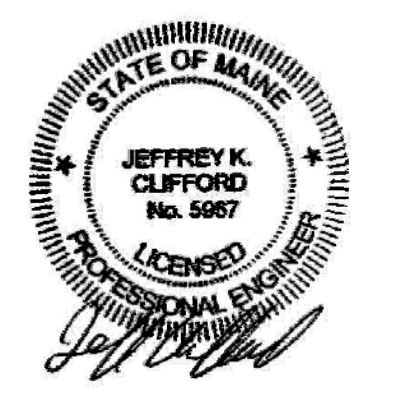
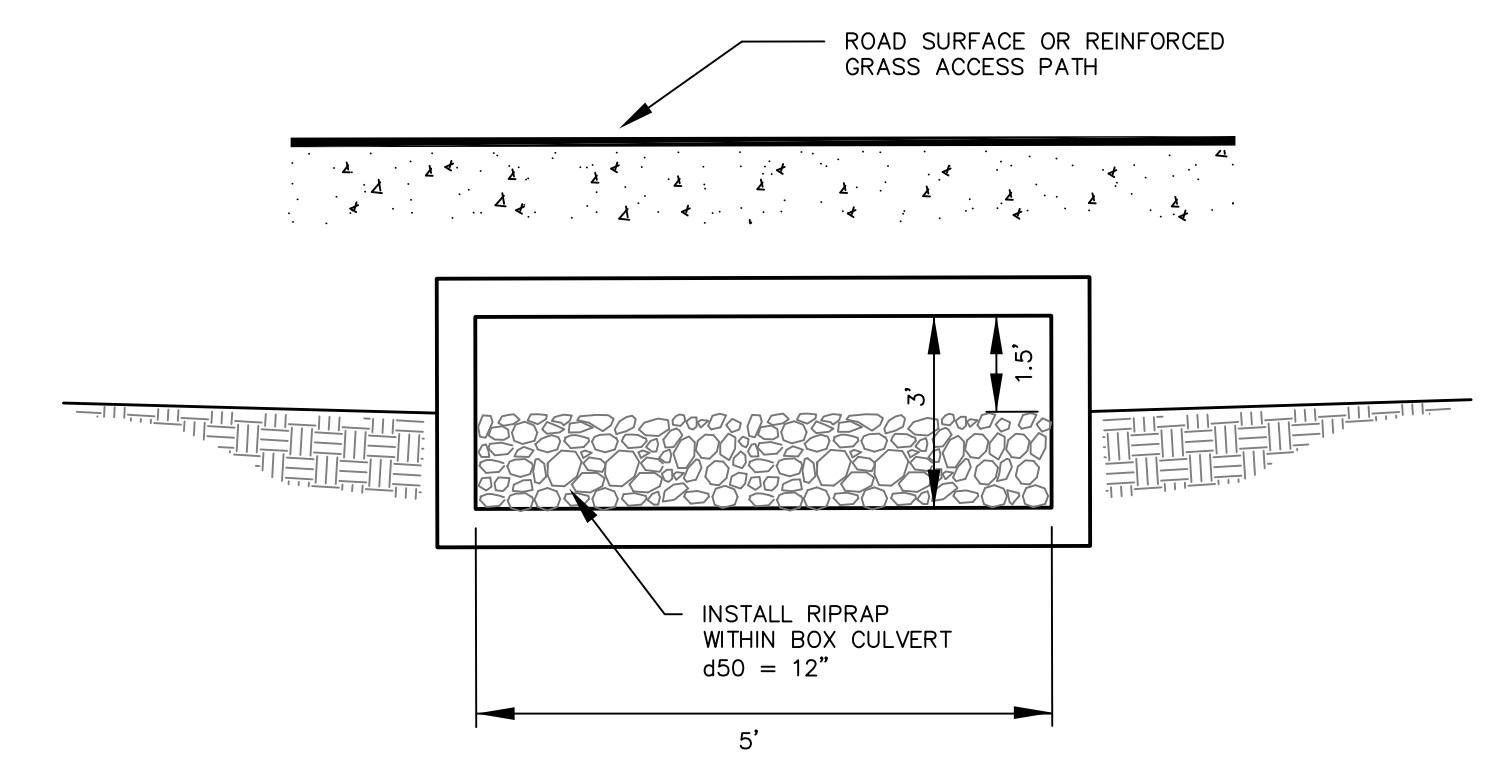


**TYPICAL ROADWAY CROSS SECTION**  
@ LOOP CUL-DE-SAC  
NOT TO SCALE



**PLAN**

**CULVERT DETAIL AT WETLANDS X-ING**  
NOT TO SCALE



**THIS DRAWING HAS NOT BEEN RELEASED FOR CONSTRUCTION**

**ISSUED FOR:** MODIFICATION OF APPROVAL  
**ISSUE DATE:** MARCH 21, 2019

NO.	DESCRIPTION	BY	DATE
0	PB SUBMISSION	JKC	6/22/17
1	MDEP SUBMISSION	JKC	3/19/18
2	REVISED ROADWAY X-SEC	JKC	8/23/18
3	TOWN FINAL APPROVAL	JKC	1/24/19
4	REVISE ROAD X-SECTION	JKC	3/21/19

**DRAWN BY:** RMB  
**APPROVED BY:** JKC  
**DRAWING FILE:** 4567SITE.DWG

**SCALE:** N.T.S.

**OWNER/APPLICANT:**  
**CHINBURG DEVELOPMENT, LLC**  
 3 PENSTOCK WAY  
 NEWMARKET, NH 03857

**PROJECT:**  
 HUNTINGTON RUN  
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 MAP 66 LOTS 2A, 8 & 8A  
 40 BETTY WELCH ROAD  
 KITTERY, MAINE

**TITLE:**

**DETAIL SHEET**

**SHEET NUMBER:**

**C-6.0**